

Historic Structures Survey for the Washington Pike Roadway Improvements Project

Knoxville, Knox County, TN

TDOT PIN # 043090.00

SUBMITTED BY:

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City of Knoxville, Tennessee
PO Box 1631
Knoxville, TN 37901

JANUARY 2013

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for the
Washington Pike Roadway Improvements Project
in Knoxville, Knox County, TN**

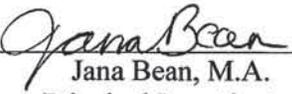
Submitted by
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Pursuant to 36 CFR 800 and Section 4(f) Evaluation

Prepared for
City of Knoxville, Tennessee
PO Box 1631
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Lead Federal Agency:
Federal Highway Administration
TDOT PIN # 043090.00

January 2013



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Management Summary

CDM Smith conducted the historic structures survey for proposed improvements to Washington Pike in the City of Knoxville in Knox County in compliance with Section 106 of the National Historic Preservation Act of 1966, as amended, and Section 4(f) of the U.S. DOT Act of 1966. This survey was conducted on behalf of the City of Knoxville and the Tennessee Department of Transportation with funding from the Federal Highway Administration. The project is located along Washington Pike with its western terminus at the I-640 interchange and its eastern terminus at Murphy Road.

A search of the Tennessee State Historic Preservation Office files revealed no resources listed on the National Register of Historic Places (NRHP) in the general vicinity of the project. A historic structures survey was conducted in April 2012 to identify historic resources in the designated project Area of Potential Effect (APE), determine their eligibility for listing on the NRHP, and assess the project's potential effect on eligible properties.

Results of the recent field survey found 14 resources within the APE of which 13 resources were determined not eligible and one resource is recommended eligible for the NRHP. It is the opinion of the consultant that the Murphy Springs Farm (KN-2586) is eligible for the NRHP based on Criteria A as an example of a family farmstead in the Early Settlement of Knox County and Criteria C for its example of Gothic Revival architecture. This property was examined for potential effects by the project. It is the opinion of the consultant that the project as proposed will not impact the NRHP-eligible resource and therefore, the project will have no adverse effects to historic properties under Section 106. Therefore, there would be no Section 4(f) use of a historic property.

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Introduction

CDM Smith conducted the historic structures survey for the proposed widening of Washington Pike Road in the City of Knoxville in Knox County. The survey was conducted in April 2012 to identify historic properties in the designated Area of Potential Effects (APE), determine the eligibility of historic properties for the National Register of Historic Places (NRHP), and assess the project's potential effect on eligible properties. This survey was conducted, as is required of the Tennessee Department of Transportation (TDOT) and the Federal Highway Administration (FHWA), in compliance with Section 106 of the National Historic Preservation Act of 1966 (NHPA) as amended, and Federal Regulation 36 CFR 800. If it is determined that the proposed project would have an adverse effect to a historic property, then FHWA provides the Advisory Council on Historic Preservation (ACHP) an opportunity to comment on the effect.

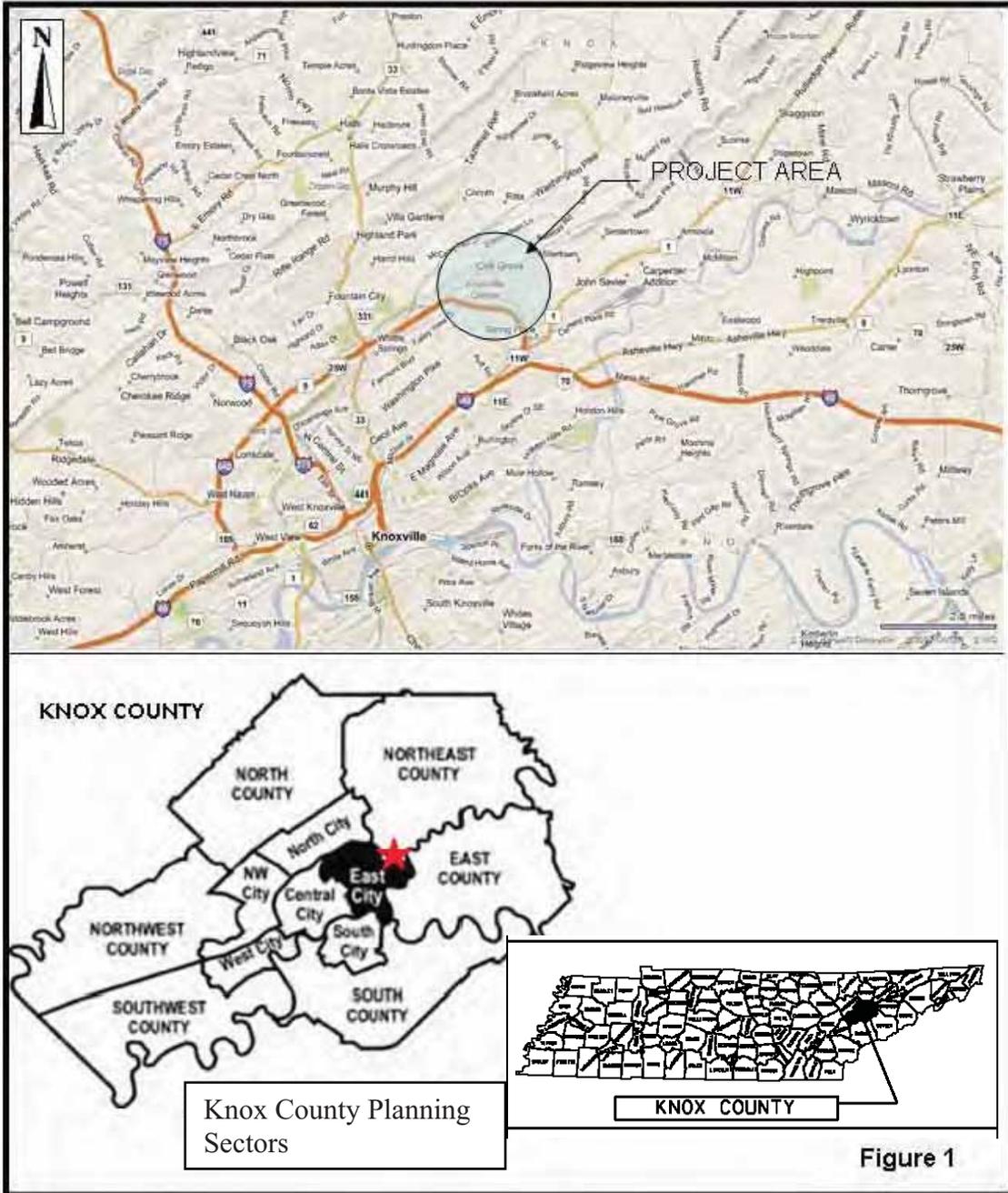
FHWA also is required to assess the applicability of Section 4(f) of the U.S. Department of Transportation Act of 1966, as amended. The project may not "use" an historic property unless there is no prudent and feasible alternative to that use and unless the project includes all possible planning to minimize harm to an historic property. Section 6009 of SAFETEA-LU (Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users) was implemented in 2005 to take into account any avoidance or minimization of impacts along with any mitigation or enhancement measures to determine the extent of the impacts to the property. Section 4(f) will be satisfied if it is determined that a transportation project will have only a *de minimis*, or minimal, impact to the historic property.

Project Description

The project is located along Washington Pike with its western terminus at I-640 and its eastern terminus at Murphy Road (see Figure 1 for project location). Washington Pike begins as a four-lane, divided roadway at its intersection with I-640 then turns east at Greenway Drive and quickly tapers to a two-lane roadway with no shoulders. There are traffic signals at Greenway Drive, Mill Road, and Murphy Road.

Proposed improvements to Washington Pike would consist of widening to four, twelve-foot traffic lanes (two lanes in each direction) with turn lanes as required at the intersecting side streets, and the installation of two-foot curb and gutter, five-foot sidewalks, and four-foot bike lanes. The proposed corridor is 200 feet in width and extends for 1.73 miles. Roadway realignment of Washington Pike would occur at the approaches to intersections with Mill Road and McCampbell Drive to correct roadway deficiencies. Additional turn lanes would be incorporated at Greenway Drive, Rising Oaks Way, Mill Road, Steeple Shadow Way/Babelay Road, Aylesbury Drive, Edmondson Lane, Trestle Way, McCampbell Drive, and Murphy Road. The project would also include cut and fill and retaining walls where needed. The purpose of the widening of Washington Pike project is to provide a transportation facility that enhances mobility, supports economic development, improves safety, provides alternate modes of travel, and relieves traffic congestion.

PROJECT VICINITY MAP



Area of Potential Effect

Pursuant to 36 CFR 800 regulations, an Area of Potential Effect (APE) was identified to determine if the proposed project would affect historic resources included in or potentially eligible for the NRHP. An APE is defined in 36 CFR 800.16 (d) as:

the geographic area or areas within which an undertaking may directly or indirectly cause changes in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking.

The proposed project is located in a mixed-use area just inside city-limits that includes commercial, rural residential, and new residential subdivisions. The nature of this project includes roadway widening and the addition of sidewalks, curb and gutter, and bike lanes. This led to an APE that takes into account changes in air quality, noise levels, setting, and land use.

The area of potential effect for this project includes the following:

- Parcels adjacent to the project that may be directly impacted;
- Areas within the viewshed of the project as bounded by tree lines or other obstructions to account for changes in setting, and;
- Areas within the potential noise impact area which includes up to 500 feet from the proposed improvements.

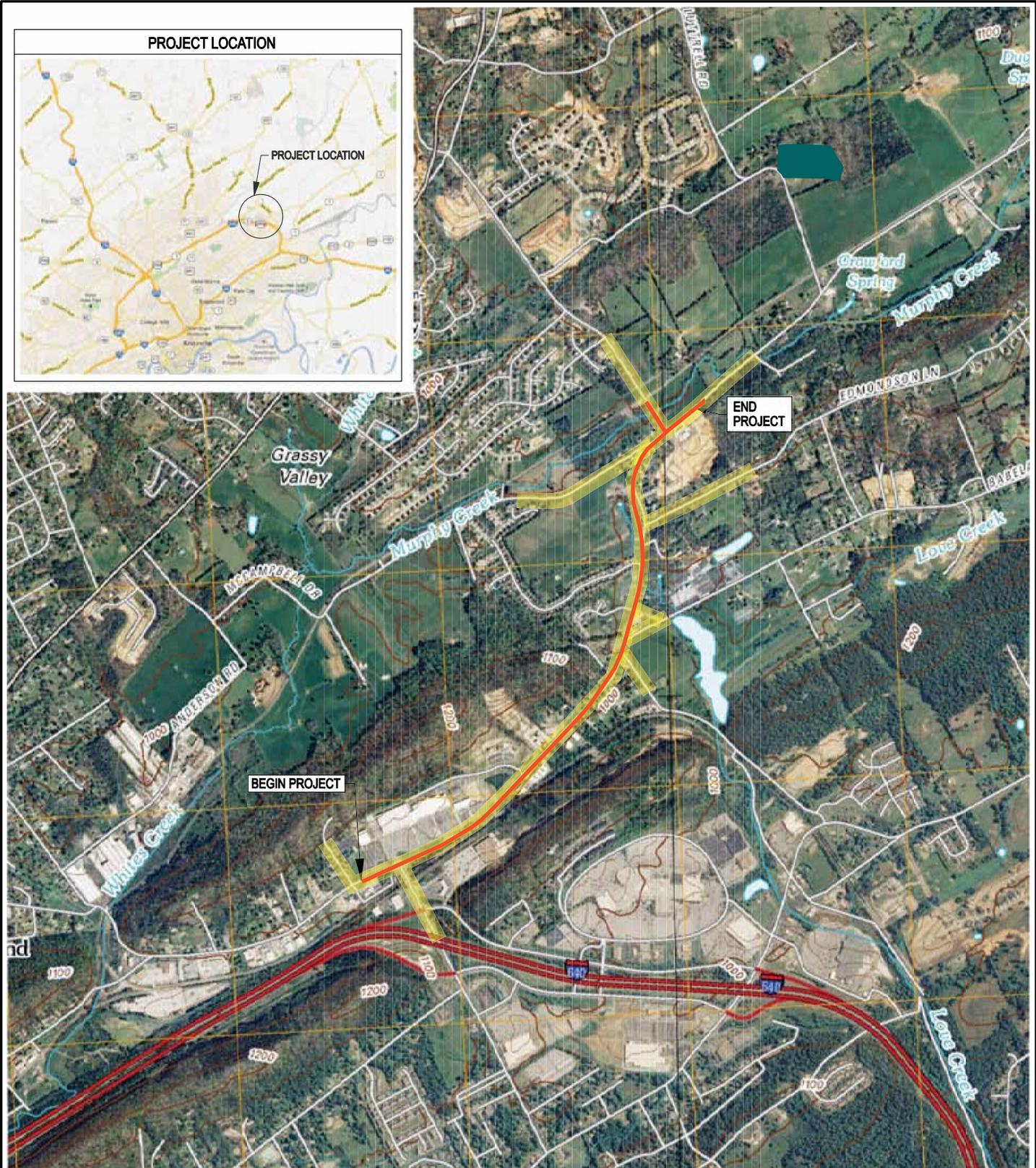
(See Figure 2 for a map of the APE)

Methodology

A literature review was conducted at the Tennessee State Historical Preservation Office (TN-SHPO) to identify previous surveys conducted in the area and any resources listed or eligible for listing on the NRHP in the vicinity of the project. The review revealed no resources listed on the NRHP in the project vicinity.

The Knoxville/Knox County Metropolitan Planning Commission conducted a historical and architectural survey of the County between 1982 and 1984 which resulted in a National Register multiple property listing, *Historic and Architectural Resources in Knoxville and Knox County, Tennessee* (Bennett 1994). (Refer to Appendix C for a map.) Three properties were surveyed that are within or near the project APE, the Babelay House (KN-2566), the Murphy House (KN-2586), and the LeCoultré House (KN-2568). Refer to Figure 5 for location of these properties. The Babelay House and the Murphy House were determined eligible and the LeCoultré House was determined not eligible. The Babelay House is not within the APE for this project due to obstructions to the viewshed. The Murphy House and the LeCoultré House are within the APE and are evaluated later in this document.

In 2000 Thomason and Associates conducted an architectural survey of Tazewell, Washington and Millertown Pikes for a TDOT Advanced Planning Report. (Refer to Appendix C for a map of surveyed properties.) Of the properties surveyed that are within the APE for this project, ten properties, of which seven are extant, were recommended not eligible to the National Register. The seven extant surveyed properties are KN-11040, KN-13239, KN-13237, KN-13232, KN-13231, KN-13230, KN-2568/13234. The survey also recommended that the Babelay House and the Murphy House were no longer eligible for the National Register due to deterioration and neglect of the Babelay House and lack of integrity of its original design of the Murphy House, although this design was present during the 1984 survey.



LEGEND

- AREA OF POTENTIAL EFFECT (APE) -AREAS DRIVEN
- PROPOSED ALIGNMENT

USGS Quadrangle Maps
7.5 Minute Series

Fountain City (2010)
&
John Sevier (2010)

FIGURE 2 -- APE MAP WITH PROJECT ALIGNMENT

A field survey was conducted in April 2012 to identify historical resources that may be eligible for the National Register in accordance with National Register Criteria A, B, and C (36 CFR Part 60.4). The field survey revealed 14 properties that were inventoried and evaluated according to National Register criteria. Historical research was conducted at the Tennessee Historical Commission, the McClung Collection at the Knoxville County Public Library, and the University of Tennessee-Map Library to review the history of the area and develop a historic context in which to evaluate the historical significance of these resources. Property owners were interviewed when possible to obtain any pertinent information concerning their respective properties. Documentation for historic resources included color digital photography and notation on the *Fountain City, Tennessee* and the *John Sevier, Tennessee* 7.5 minute USGS topographic maps. In the opinion of the consultant, one inventoried property, the Murphy Springs Farm (KN-2586), meets the eligibility criteria for inclusion in the NRHP.

The eligible property was also evaluated for the potential for impacts by the proposed project in accordance with 36 CFR 800. In the opinion of the consultant, the project as proposed will have no adverse effect to the eligible historic property. Therefore, there will be no Section 4(f) use of a historic property.

Public Participation

The current project is Segment Two of a study developed in 2001 by the City to improve traffic conditions and accommodate future growth in the areas of the Knoxville Center Mall and I-640. The larger study involved four segments:

- Segment One- Widen Millertown Pike from Mill Road to I-640
- Segment Two - Widen Washington Pike from I-640 to Murphy Road
- Segment Three- Widen Washington Pike from I-640 to Millertown Pike
- Segment Four- Widen Millertown Pike from I-640 to Washington Pike

Working Group meetings were held with interested parties on July 18 and October 9, 2006 to discuss improvements to Washington Pike and Millertown Pike. Representatives were from the Alice Bell-Spring Hill Association, Knoxville Center Mall Area Businesses, Knox County Metropolitan Planning Commission, Northeast Knox Preservation Association (NEKPA), Fountain City, Knox County, and the City of Knoxville.

Comments from groups representing historical interests were as follows. Alice Bell-Spring Hill Association was supportive of improvements south of I-640 which is the area utilized by their residents most. NEKPA expressed concern for placing priority on improvements north of I-640. Fountain City expressed support of extending Murphy Road to alleviate Tazewell Pike traffic.

On August 17, 2012, TDOT mailed letters to five groups representing Native American interests and asked them if they wished to participate in the historic review process as consulting parties. Letters were sent to the following:

Tyler Howe
Eastern Band of Cherokee Indians

Lisa LaRue-Baker
United Keetoowah Band of Cherokee

Richard Allen
Cherokee Nation

Robin Dushane
Eastern Shawnee Tribe of Oklahoma

Kim Jumper
Shawnee Tribe

No responses were received. Copies of the consulting party invitation letters are in Appendix B.

Appendix B also contains a list of historic groups, county historians, and other such individuals or organizations that might be interested in the proposed project. A copy of this report will be mailed to these interested groups and individuals.

The City may choose to host a public meeting upon completion and approval of the Categorical Exclusion document and development of Preliminary Roadway Plans.

Environmental Setting

Knoxville lies in the Ridge-and-Valley physiographic region in eastern Tennessee which is between the Appalachian Plateau to the west and the Blue Ridge Mountains to the east. The long ridges and corresponding valleys lie generally northeast to southwest. Cultivation typically has occurred in the valleys whereas the ridges have remained forested. Water sources in the area include the Holston and French Broad Rivers which come together to form the Tennessee River at Knoxville. Numerous creeks feed the Tennessee River including First and Second Creeks. First Creek comes from the north of downtown Knoxville with White's Creek as a tributary from the east. Murphy Creek extends eastward off of White's Creek. Both feed the Grassy Valley area that is between Black Oak Ridge to the north and Sharp Ridge to the south. The Grassy Valley area is so named for the lush grasses located between the steep slopes of the ridges. This was an excellent area for agricultural development.

The project is in an area that is commercial at the west end and rural residential at the eastern end. At the west end the project begins at the interchange of Washington Pike and Interstate-640, which curves around Knoxville as a bypass. Washington Pike has seen a rise in commercial development in recent years at this location. Continuing eastward, the scene changes to rural residential with primarily mid-century housing on one-acre plots. New subdivisions have been constructed leading off of Washington Pike as the road continues east of Mill Road. The project area's eastern end has a large farm, convenience stores, and a 1970s development. The project ends at the Knoxville city limits on Murphy Road.

Historical Overview

Early Settlement

Knoxville lies in the ridges and valleys west of the Appalachian Mountains. The ridges are on a northeast to southwest axis which made crossing from the eastern colonies to newly opened lands in the west difficult. Nevertheless, by the time of the Revolutionary War, settlers had begun trickling over the mountains to settle along the river valleys of east Tennessee. The city of Knoxville grew up along the north bank of the Tennessee River just west of the confluence of the Holston and French Broad Rivers that form the Tennessee. Knoxville was actually the capital of the territory and then state of Tennessee until 1812. However, due to the difficulties in travel in the region, Knoxville grew slowly. The local economy was based on serving the immediate area and did not develop industries to serve the region. The surrounding topography of valleys and mountains made transportation of goods difficult. Small, relatively subsistent farms were the norm as opposed to the large plantations found elsewhere in the South (Bennett 1994).

Overland roadways such as Tazewell and Washington Pikes were established radiating from

Knoxville to burgeoning communities in the region. Tazewell Pike extended to the northeast to the community of Tazewell with access to nearby Cumberland Gap and Washington Pike also led northeast towards Washington County, Virginia just across the border. After the Civil War, Tazewell Pike was one of five roads chosen that led out of Knoxville to be improved as a toll road (Knoxville/Knox Co. MPC 2007). Several of the pikes located north of the city connected to North Broadway which led straight into downtown.

In 1848, at the invitation of the German-American East Tennessee Colonization Company, Swiss settlers arrived in the Knoxville area. Over the years, many families settled northeast of Knoxville and established farms. By 1850, the Swiss were the largest ethnic group of the new settlers in the area. One of these families, the Babelays, settled along Washington Pike and eventually established a large greenhouse business (Babelay 2009).

Industrial Growth

In 1855 the East Tennessee Valley and Georgia Railroad was constructed leading north out of Knoxville along Second Creek towards Bristol, Tennessee. When the Civil War began, Knoxville was seen as important to the Union effort due to the railroad. This line was a link between Virginia and the Mississippi River and used for transportation of troops and support goods (Sammartino 1996). To achieve control of the rail line, Union forces under Major General Ambrose Burnside occupied Knoxville by September 1863 after a short siege of the city from the north. Undaunted, Confederate forces under General James Longstreet lay siege to Knoxville that November but by early December had withdrawn leaving the city in the hands of the Union occupiers. The Civil War brought no serious destruction to the city and surrounding communities as in other parts of the region.

A result of Union occupation was the attention brought to Knoxville's resources to those in the Union army occupying the city. Several who had capital to invest came back after the war to begin Knoxville's industries (Bennett 1994). Industry in Knoxville was made possible due to its railroad connections. Service had been disrupted during the Civil War, but once restored it became the impetus to growth for areas north of downtown Knoxville. The creeks that feed into the Tennessee River acted as a water source to provide power to the factories. Also, as more railroads were constructed intersecting Knoxville, the city became a center in the region for wholesale businesses (Brown 1980).

One of the connecting rail lines constructed after the war was the Powell's Valley Railroad that was begun in 1887. This line led northeast out of Knoxville, paralleling Washington Pike, and connected to Middlesboro, Kentucky near the Cumberland Gap. This was a coal mining area of Kentucky and therefore the Powell's Valley line brought coal back to Knoxville for use in the iron foundries. It also provided coal to communities along the rail line. The line eventually became the Knoxville, Cumberland Gap and Louisville Railroad before being incorporated into the Southern Railway (Rule 1900). The line is now owned by Norfolk Southern. A bypass line that connected to this line was constructed in the early 1920s around the eastern edge of the city to the new John Sevier railyards. The community of Beverly, just west of the project and located at the juncture of these two lines, developed warehousing to service the rail lines.

Residential Growth

Manufacturing did not come to outlying areas along Washington Pike. Instead, the area was home to two known greenhouse businesses. As mentioned, the Babelay greenhouses were located along Washington Pike and Babelay Road. Another greenhouse business was Charles Baum's Home of Flowers established in 1889 along Tazewell Pike (Knoxville/Knox Co. MPC 2007). These two businesses grew the exotic and delicate flowers that were popular in the Victorian gardens of the

wealthy and upper middle-classes who were building new homes in the new suburbs of Knoxville. With the rise of new factories on the outskirts of Knoxville came the construction of neighborhoods to house the workers, managers, and owners of the new factories. Two such neighborhoods that contained the larger Queen Anne style homes and gardens were Fourth and Gill for middle-class professionals and Old North Knoxville which had more of the owner-class homes. Also, larger estates were established along Tazewell Pike leading away from the new suburban areas.

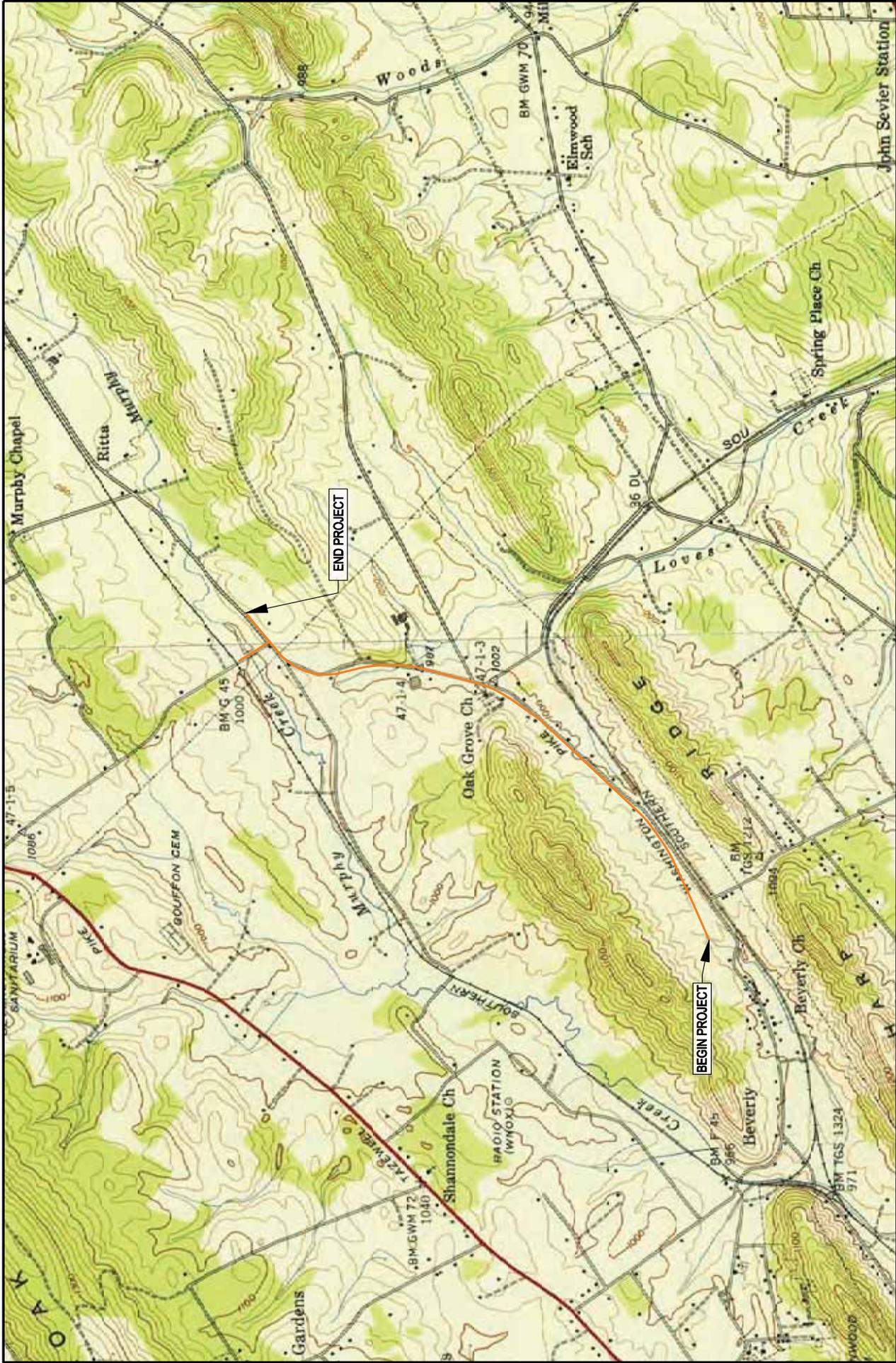
Streetcar lines, such as the Dummy Line that led to Fountain City along North Broadway, enabled the growth of these residential areas and attracted not just homes but businesses to serve the residences as well as churches and schools. Fountain City, so named for the fresh water springs, was the site of early camp grounds for the Methodist Church. By the 1880s the site became a health resort with a hotel, park, and lake. To reach the resort, a street car line, the Dummy Line since it was not a real rail line, was established in 1890. By the 1920s the area had become a commuter suburb with the coming of the automobile (Bennett 1984).

Another community that arose in the area was an African American community centered on the Oak Grove AME Zion Church that was established in 1868 at the corner of Washington Pike and Mill Road (as seen in Figure 3). It is unknown if this was the original location for the church but a building was constructed at this intersection in 1915 with the existing building constructed in 1926. AME Zion (African Methodist Episcopal) churches were established by missionaries in the southern states after the Civil War. Rural African American churches were frequently the focal point of community gatherings and social activism. Sometimes schools and fraternal lodges were built nearby (MTSU 2000). Several African American families associated with the Oak Grove church purchased land nearby along Washington Pike. For instance, William T. Johnson's land was located west of the intersection of Washington Pike and Mill Road. Upon his death in 1946, his land was divided into several parcels among his descendants who then built their own homes. The land has continued to be passed down to succeeding generations. Mr. Johnson's home is no longer extant.

The result of the spreading residential development was that by the mid-twentieth century, the farms located along the old pike roads that radiated from Knoxville were being replaced by subdivisions that could be reached by automobile along the pikes. The demand for housing, especially after World War II, accelerated the transformation of the farmland into residences (refer to Figure 4) (Knoxville/Knox Co. MPC 2007).

When I-640 was constructed in the 1970s around eastern Knoxville, the area began to take on a suburban feel. New subdivisions were constructed and the area of East Knoxville underwent revitalization. The Knoxville Center Mall was opened in 1984 with access from I-640 at Millertown and Washington Pikes. The area has continued to attract new commercial establishments and subdivisions with an increase in the past decade. Farms have been subdivided for the new subdivisions, further reducing the rural feel of Washington Pike in this area.

Following are historic topographic maps (Figures 3 and 4) that show the progression of development in the area with the project alignment overlaid along Washington Pike. The project alignment as shown reflects the current alignment of the roadway and not the historical alignment. The maps, including the map for Figure 5, do not reflect the northwesterly realignment of the west end of Washington Pike at Greenway Drive that occurred with the construction of I-640 in the 1970s. Project alignment departures from the roadway along the curves at the eastern end of the project reflect a proposed realignment of the roadway near McCampbell Road.



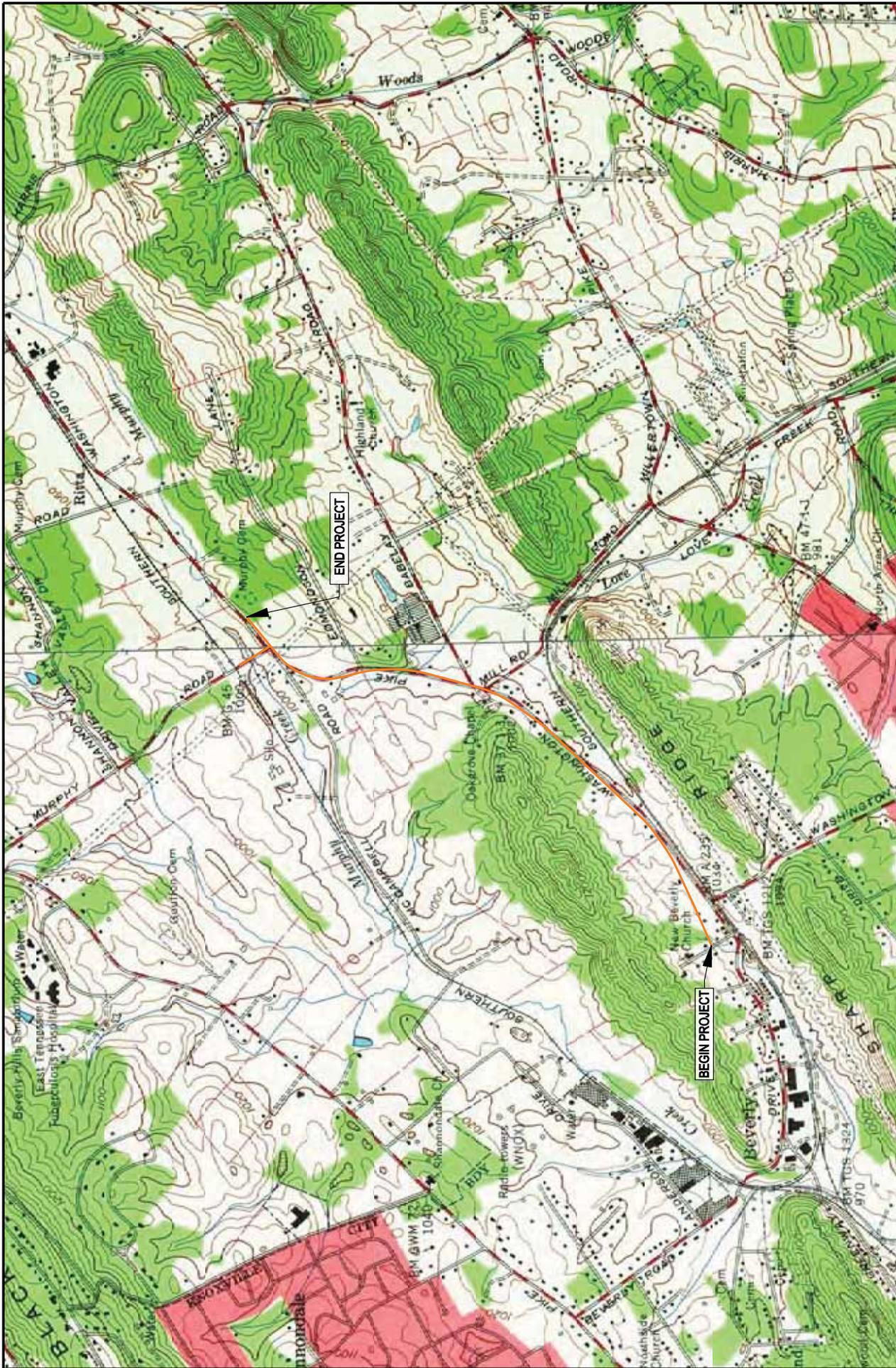
USGS Quadrangle Maps
7.5 Minute Series

Fountain City (1941)
&
John Sevier (1940)
(Reprinted 1950)

LEGEND

— PROPOSED ALIGNMENT

FIGURE 03-- HISTORIC TOPOGRAPHIC MAPS



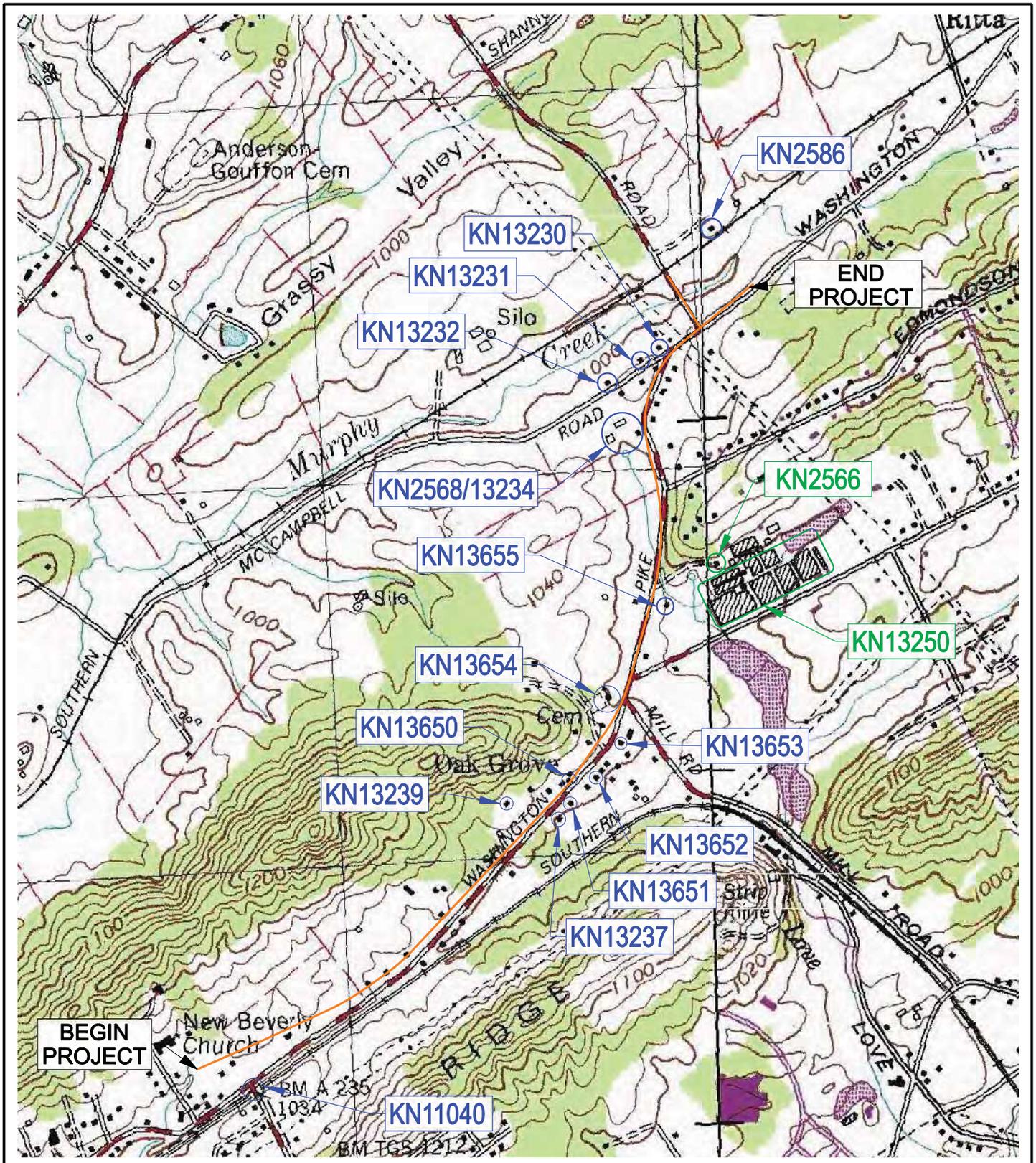
USGS Quadrangle Maps
 7.5 Minute Series
 Fountain City (1966)
 &
 John Sevier (1966)

FIGURE 04-- 1966 TOPOGRAPHIC MAPS

LEGEND

— PROPOSED ALIGNMENT

N



LEGEND

-  PROPOSED ALIGNMENT
-  SURVEYED PROPERTY
-  PREVIOUSLY SURVEYED PROPERTY

USGS Quadrangle Maps
7.5 Minute Series

Fountain City (1978)
&
John Sevier (1978)
(Photorevised 1992)

FIGURE 5 – MAP OF SURVEYED PROPERTIES

Field Survey Results

KN-11040

Old Washington Pike Bridge

Constructed c. 1930, this resource is a reinforced concrete beam bridge that carried a two-lane asphalt road, Old Washington Pike, over the former Southern Railroad tracks that connected to the John Sevier railyards. The bridge has two reinforced concrete piers and concrete abutments. The railings on top of the deck are steel with square concrete balustrades. It is located parallel to Washington Pike, crossing over the Norfolk Southern Railroad north of I-640. Use of the bridge was discontinued in the 1970s when Washington Pike was realigned due to the construction of I-640.

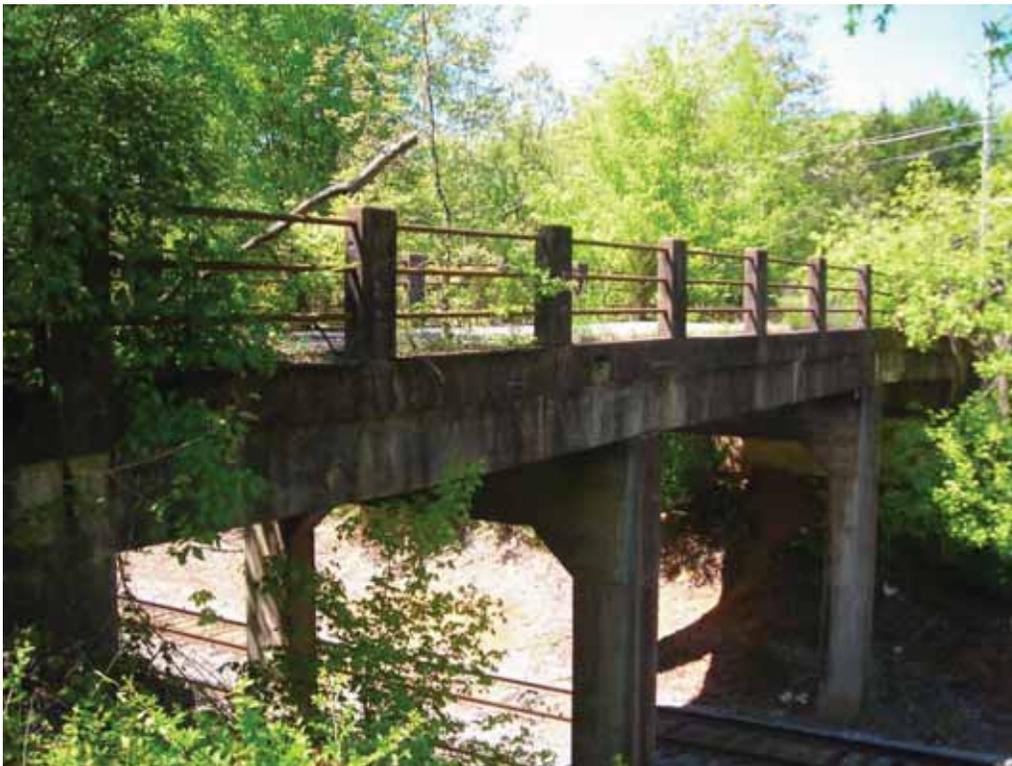


Figure 6 – KN-11040, east elevation

The bridge is a common type of concrete beam highway bridge of the 1930s era and does not display any significant architectural or engineering features that would qualify it as eligible under Criteria C. The bridge has no known associations with significant persons or events that would qualify it as eligible under Criteria A or B. KN-11040 is recommended not eligible for the National Register.

KN-13239
5609 Washington Pike

Situated on the north side of Washington Pike and facing south, this is a one and a half story frame house constructed c. 1925 in the Craftsman style and rests on a brick pier and concrete block foundation. The house is sheathed in vertical board panels and there are stamped metal panels skirting most of the foundation. The front façade has double entry doors in the center flanked by two 2/2 sash windows and two picture windows to either side. A second entry door is located on the left side and leads to an enclosed porch. Other windows on the house are 2/2 horizontal. The full-width front porch has vinyl columns and new square post railings and steps. The side gable roof has asphalt shingles, a large shed dormer with two 2/2 windows, and a brick chimney on the ridge. There is also a brick chimney flue on the exterior west elevation. The rear façade (north elevation) has a shed dormer with a row of aluminum sash windows. An enclosed walkway has been added to the rear to connect to an open three-bay garage.

There is one outbuilding, an original shed, located to the west that rests on rock and wood piers and is sheathed in horizontal boards with a standing seam metal roof and exposed rafters. There is a window on the west elevation and a door on the east elevation.



Figure 7 – KN-13239, south elevation

The house is currently rented and there are four small businesses on the property close to the roadway. The Craftsman-style house underwent several unsympathetic changes in the 1970s including the addition of the picture windows, vertical siding, enclosed porch, and double entry doors. Under Criteria A or B, KN-13239 has no known associations with significant persons, events, and does not retain sufficient architectural integrity of the Craftsman style features under Criteria C; therefore, KN-13239 is recommended not eligible for the National Register.

KN-13237
5608 Washington Pike

Situated on the south side of Washington Pike and facing north, this is a one-story frame house constructed c. 1930 in the Bungalow style and rests on a brick foundation covered by concrete stucco. This front gable house has asphalt shingles, exposed rafters, open eaves, and an interior brick chimney on the east elevation and an exterior brick chimney on the rear. The walls are sheathed in asbestos shingles. The engaged porch is partial-width with a paneled entry door and screen door and a 1/1 window. The porch has wood tapered piers on brick veneer columns and iron railing. The right side of the porch has been enclosed and contains a paneled entry door on the east elevation and a picture window with 1/1 sash sidelights on the façade that is surrounded by asbestos shingles and a brick veneer skirt wall under the window. To the rear on the west elevation is a side entry with iron steps that leads into an original shed porch. The shed porch has vinyl siding and a row of screened windows. The house has a partial basement.

There are two outbuildings, a concrete block garage and a concrete block shed. The garage has a gable, standing seam metal roof, paneled side door and metal garage door. The shed has a vertical paneled door, three-pane window, and a gable, corrugated tin roof.



Figure 8 – KN-13237, north elevation

The house is currently owned by Alfred Nance, a descendant of Josie Crippen, who received the property in 1955 according to tax records. The Crippen family was active members of nearby Oak Grove AME Zion Church (KN-13654) at the time of the 1926 construction. The Bungalow-style house underwent several alterations in the late 1950s including the addition of the picture window and enclosed porch, and windows. Additional alterations since the 19050s include covering the rear porch with vinyl siding, addition of iron steps to the rear entry door, and stuccoing the brick foundation. Under Criteria A or B, KN-13237 has no known associations with significant persons, events, and does not retain sufficient architectural integrity of the Bungalow-style features under Criteria C; therefore, KN-13237 is recommended not eligible for the National

Register.

KN-13650

5621 Washington Pike

Situated on the north side of Washington Pike and facing south, this is a one-story frame house constructed c. 1950 in the Ranch style. It has a side gable roof with asphalt shingles and has been sheathed in vinyl siding. The porch is an entry stoop that has been gated with iron fencing to form a patio. The door is a replacement and there are brick pilasters to either side that extend three-fourths of the height of the door. The windows throughout the house are 1/1 single pane with vinyl muntins forming a 6/6 pattern. There is a picture window with 1/1 sash sidelights and a partial brick surround to the right of the entry door. The house rests on concrete block foundation and has open, close eaves. An ell addition extends to the rear with rear entry door and a small concrete block shed has been added to the rear of this addition.



Figure 9 – KN-13650, southwest elevation

According to Isom Jamison, the owner is Theodora Jamison who currently rents the property to family members. Theodora's mother, Elizabeth Isom, inherited the land from the Johnson estate in 1946 according to tax records, and presumably lived here until her death in 1997. The Johnson family has been longstanding members of the Oak Grove AME Zion Church (KN-13654). The house has been altered with the addition of vinyl siding, closure of the entry porch, and unsympathetic additions to the rear. Under Criteria A or B, this house is not associated with a significant person or event, and does not possess significant architectural features of the Ranch style under Criteria C; therefore, KN-13650 is recommended not eligible for the National Register.

KN-13651
5610 Washington Pike

Situated on the south side of Washington Pike and facing north, this is a one-story frame house constructed c. 1960 in the Ranch style. It has a side gable roof with side gables wings to either side that are set back from the main façade. An extended roof covers the porch along the main façade and has square wooden supports and concrete slab flooring. The paneled door is new and there are 1/1 sash windows to either side with vinyl muntins forming a 9/6 pattern. The side gable wings each have a picture window with the one on the right having 1/1 sidelights. The front facade has brick veneer and the rest of the house is sheathed in vinyl. The gables in the main façade have masonite siding. There is a large exterior brick chimney on the west elevation. There are two cross gables extending to the rear. The cross gable to the west was a porch that has been partially enclosed and has a vinyl entry door leading to a wooden deck. The rest of the porch has framed screening with side entryway. The cross gable to the east has a sliding glass door leading to the wooden deck.

There is one outbuilding, a concrete block garage to the rear of the property. The garage has a new aluminum roll door, a 1/1 window, and a new vinyl door. There is a pence roof above the door. The gable roof is extended and has particle board and bracing in the eaves.



Figure 10 – KN-13651, north elevation

The house has been altered with the addition of vinyl siding and unsympathetic additions to the rear. Under Criteria A or B, this house is not associated with a significant person or event, and does not possess significant architectural features of the Ranch style under Criteria C; therefore, KN-13651 is recommended not eligible for the National Register.

KN-13652
5624 Washington Pike

Situated on the south side of Washington Pike and facing north, this is a one-story frame house constructed c. 1960 in the Ranch style and rests on a concrete foundation. There is a brick chimney on the ridge. The house has a side gable roof with asphalt shingles and an extended front cross gable on the left side of the facade. The front façade has a shed roof entry porch with new paneled door, concrete steps and decorative iron railing. The cross gable and entry have brick veneer which extends across the rest of the façade as a skirt wall. There is vertical siding above the skirt wall and the rest of the house has asbestos siding. There is a picture window with 2/2 sash sidelights to the right of the entry door. The rest of the house has 6/6 paired windows. A side entry on the west elevation has concrete steps, iron railing, and a vinyl awning. On the east elevation is a new sliding glass door leading out to a new deck. There is also a sliding glass door on the rear that leads to a broad deck and a sliding glass door that leads out from the basement. Also on the rear façade is an exterior concrete block chimney flue and the windows in the basement are 1/1 horizontal.

There is one outbuilding on the property, a concrete block garage that has a gable roof with asphalt shingles. The two garage doors are aluminum roll doors.



Figure 11 – KN-13652, north elevation

According to the current owner, Mark Isom, he bought the property from Marion Wells in 2011 who had received the property in 1946 from the Johnson Estate. The house has been altered with the addition of the three sliding glass doors. Under Criteria A or B, this house is not associated with a significant person or event, and does not possess significant architectural features of the Ranch style under Criteria C; therefore, KN-13652 is recommended not eligible for the National Register.

KN-13653
5650 Washington Pike Road

Situated on the south side of Washington Pike and facing north, this is a one-story house constructed c. 1952 in the Ranch style and rests on a concrete block foundation. The low-pitched roof is a side gable with asphalt shingles. The house is sheathed in synthetic siding and has a rock veneer skirt wall on the front façade. The rock veneer covers the wall to the right of the door. There are concrete block entry steps with decorative iron railing leading to the paneled door which has three diagonal lights. The roof extends slightly over the steps and walkway. The windows on the house are 1/1 horizontal with aluminum storm windows and there is a picture window with single pane sidelights to the left of the door. On the west elevation is a double carport with concrete slab. There is a wooden ramp leading to a side entry under the carport. To the rear is a shed roof extension with sliding glass doors on the west elevation leading to a wooden deck.

There are two outbuildings, sheds, on the property. One shed is modern corrugated tin and the other is of particle board with a gambrel, asphalt-shingled roof.



Figure 12 – KN-13653, northwest elevation

The current owner is Almeta Chesney who, with Paul Chesney, purchased the property in October 1951 according to tax records. The house has been altered with the addition of synthetic siding, the sliding glass door and wooden deck. Under Criteria A or B, the house is not associated with a significant person or event, and does not possess significant architectural features of the Ranch style under Criteria C; therefore, KN-13653 is recommended not eligible for the National Register.

KN-13654
Oak Grove AME Zion Church
5667 Washington Pike Road

Situated on the north side of Washington Pike and facing south, this is a one-story church on a full basement. According to a cornerstone, the church congregation dates to 1868 and the existing 1926 structure replaces a 1915 structure. Another plaque states the church was remodeled in 1976 (see Figure 13). The church is referred to as the Fullwood Chapel, AME Zion Church on the 1926 plaque and on a 1953 USGS topographic map. By the time of the remodeling in 1976 and on the 1941 and 1966 USGS topographic maps, the current name was in use. This church and several of the properties surrounding it have long been affiliated with a small African-American community at the crossroads of Washington Pike and Mill Road.

The 1926 portion of the church is a scaled-down Greek temple style which is a long rectangle with front gable entrance and windows along both sides. The row of 1/1 windows on either side have painted glass and there is a brick chimney on the north elevation that has been cut off at the roof line. To the rear is a hipped addition that stretches around both sides. This rear addition has an entry porch on the south elevation with a concrete walk leading to wooden steps, original paneled door and metal awning. There is also a paneled entry door with metal awning on the west elevation of the addition. The basement has stucco and has windows along the north elevation.

The 1976 changes include brick veneer added to the entire structure and a gable addition to the front façade. The gable addition is on the east elevation and wraps around to the south elevation where there are double, vinyl doors. There is a ribbon of lights along the roof line and an inset vinyl cross in the brick veneer on the east elevation. There are also 1/1 windows on the lower level of the east elevation.



Figure 13 – KN-13654, southeast elevation



Figure 14 – Cornerstone Plaques

There is a cemetery along the western portion of the property to the rear of the church that extends up the hillside. One of the oldest stones dates to 1874. There are approximately 70 headstones in the cemetery with many damaged or lying down. Some of the stones are grouped in family units but most are scattered. Many of the headstones date to the 1920s and 1930s, however, this is an active cemetery.

A context was developed for evaluating African American rural churches by the Center for Historic Preservation at Middle Tennessee State University (MTSU 2000). For churches from the 1890-1945 Jim Crow era, the vernacular frame, front-gable entrance style of church building was common for rural African American churches. Other themes associated with the enforced segregation of the era include activism by congregants, nearby community buildings affiliated with the church, documentation of church history on dedication plaques, and a historic cemetery establishing an overt African American identity.

This church has no known associations with a significant person such as a well-known minister or civil rights activist or an event such as education or social activism of the 1890-1945 Jim Crow Era or the 1945-1970 Modern Era to be eligible under Criteria A or B. Under Criteria C, the 1976 renovations have compromised the front-gable entrance style common to this era so that the church does not possess significant architectural features for a religious property of this type and ethnic affiliation. For these reasons, KN-13654 is recommended not eligible for the National Register.

KN-13655
5716 Washington Pike

Situated on the east side of Washington Pike and facing west, this is a one-story frame house constructed c. 1949 according to the owner, Gene Babelay. This brick house is in the Ranch style and has a hipped roof with asphalt shingles and two cross gables extending to the front. The front door is paneled with a metal screen door and there is a multi-light bay window to the left. Other windows on the house are 6/6 sash with some single and some paired. The extended gable on the left side of the façade has a one-car garage with wood paneled roll door and row of lights at the top. There is a brick chimney on the interior and a brick chimney flue at the rear. On the north elevation is a side entry door with hipped roof, concrete steps and decorative railing. On the rear is a hipped wing with garage that leads to a full basement. There are windows at the basement level that have iron grates covering them. Also to the rear is a concrete patio area with concrete picnic table and low brick wall.

According to the current owner, Gene Babelay, the parcel has been in the Babelay family since the late 19th century. The Babelay House (KN-2566) c. 1910 and not covered in this survey, is located east of this property in a separate parcel and includes the Babelay Greenhouses business (KN-13250) established at the turn of the twentieth century (see Figure 5 for locations).



Figure 15 – KN-13655, west elevation

Under Criteria A or B, since the house is not associated with the Babelay Greenhouse business, then it is not considered associated with a significant person or event. Under Criteria C, it does not possess significant architectural features of the Ranch style. KN-13655 is recommended not eligible for the National Register.

KN-2568/13234- LeCoultre House
5820 Washington Pike

Situated on the west side of Washington Pike and oriented east, this property has a collection of three barns and a smokehouse constructed during the first half of the twentieth century for the purpose of a dairy operation. Barn 1 on the northern portion of the property has two bays open at either end and was probably used for equipment storage. The gable roof has exposed rafters and is covered in corrugated tin. Much of the board and corrugated tin siding has come off. Barn 2 is the main barn at the northwestern end of the property. It is constructed of vertical boards on a concrete block foundation. The foundation forms a basement level and has two windows on the north elevation with no glass. On the east elevation is an open bay into the basement level. There is an open bay on the north elevation and an entry door on the south elevation. There are several stalls with a hay loft above in the interior. The roof is gable with standing seam metal. Barn 3, not shown, is the milkhouse that is attached to the south elevation of the main barn. The building has a corrugated tin exterior and standing seam metal roof. There are windows on the west elevation. The east elevation is covered with vegetation. Attached to the corner of the south elevation is a concrete block gable wing with standing seam metal roof and hopper windows. In the southern portion of the property is a fallen shed with wood siding and a standing seam metal roof. Its location near a drained pond indicates it was probably a spring house to keep the milk cool. The last structure on the property in the eastern portion is a log smokehouse. Constructed in the half-dovetail method, it has particle board in the overhanging gabled eaves. A small paneled door is in the north elevation and the roof is standing seam metal.

When this property was surveyed in 1984 (KN-2568) and again in 2000 (KN-13234), the associated farm house was still standing but is no longer extant. The house was a two-story central hall built c.1880 with a c. 1930 wraparound porch. The original owner was Stoffell who had a dairy operation. Dairying was continued by the next owners, the LeCoultres, whose dairy operation was called Richelieu Dairy. The property is now bank-owned.



Figure 16 – KN-2568/13234, Barn 1-southeast elevation



Figure 17 – KN-2568/13234, Barn 2-east elevation



Figure 18 – KN-2568/13234, Smokehouse-north elevation

Under Criteria A, this property is not associated with a significant person and under Criteria B the barns do not constitute an outstanding representation of dairying in eastern Tennessee. Under Criteria C the barns do not possess significant architectural features of a farmstead. KN-2568/13234 is recommended not eligible for the National Register.

KN-13232
5817 McCampbell Drive

Situated on the north side of McCampbell Drive and oriented to the east, this one and a half story frame house was constructed c. 1925 and rests on a brick and concrete block foundation. The gable roof has asphalt shingles and close eaves. The house is sheathed in weatherboard siding and there is a brick chimney on the ridge. The entry door does not face the street but rather is on the east elevation with a cement slab patio. The door is paneled with nine lights. Most of the windows on the house are paired 2/2 sash. On the south elevation facing the street in the upper story is a single pane window with three-light windows on either side. There is decorative trim along the south elevation between the first and second levels. There is a gable dormer addition on the west elevation with a large multi-light window and a small casement window. Also the west elevation exterior has vertical board paneling. On the east elevation is a cross gabled wing with entry door and windows. To the rear is a shed addition with paneled entry door that has three lights and a metal awning covering the concrete platform. There are knee braces in the gable of the north elevation.



Figure 19 – KN-13232, southeast elevation

This property is vacant and is currently owned by Carlos, Robert, and John Campbell who are descendants of Robert M. Murphy. The house has been altered with the addition of the gable dormer and exterior paneling on the west elevation. Under Criteria A or B, the house is not associated with a significant person or event, and does not possess significant architectural features of a 1920s cottage under Criteria C; therefore, KN-13232 is recommended not eligible for the National Register.

KN-13231
5831 McCampbell Drive

Situated on the north side of McCampbell Drive and oriented to the east, this is a one-story house built c. 1940 and rests on a wood pier foundation with brick and concrete block infill. The house is sheathed in masonite and has a full-width, hipped porch. The concrete floor porch has metal supports and decorative iron railing. The gable roof is reminiscent of a saltbox gable and has asphalt shingles. The door is aluminum with a glass storm door and the windows are 1/1 sash. Some of the 1/1 windows on the house have vinyl muntins for an 8/12 or 6/6 pattern. At the rear the doorway has been enclosed. It once led to rounded concrete steps and patio. A new paneled door has been installed to the right and leads to a wood deck.



Figure 20 – KN13231, southeast elevation

Under Criteria A and B, this house is not associated with a significant person or event. Under Criteria C, this house has had several unsympathetic changes including new doors and windows, new porch, and removal of a rear door and patio configuration; therefore, KN-13231 is recommended not eligible for the National Register.

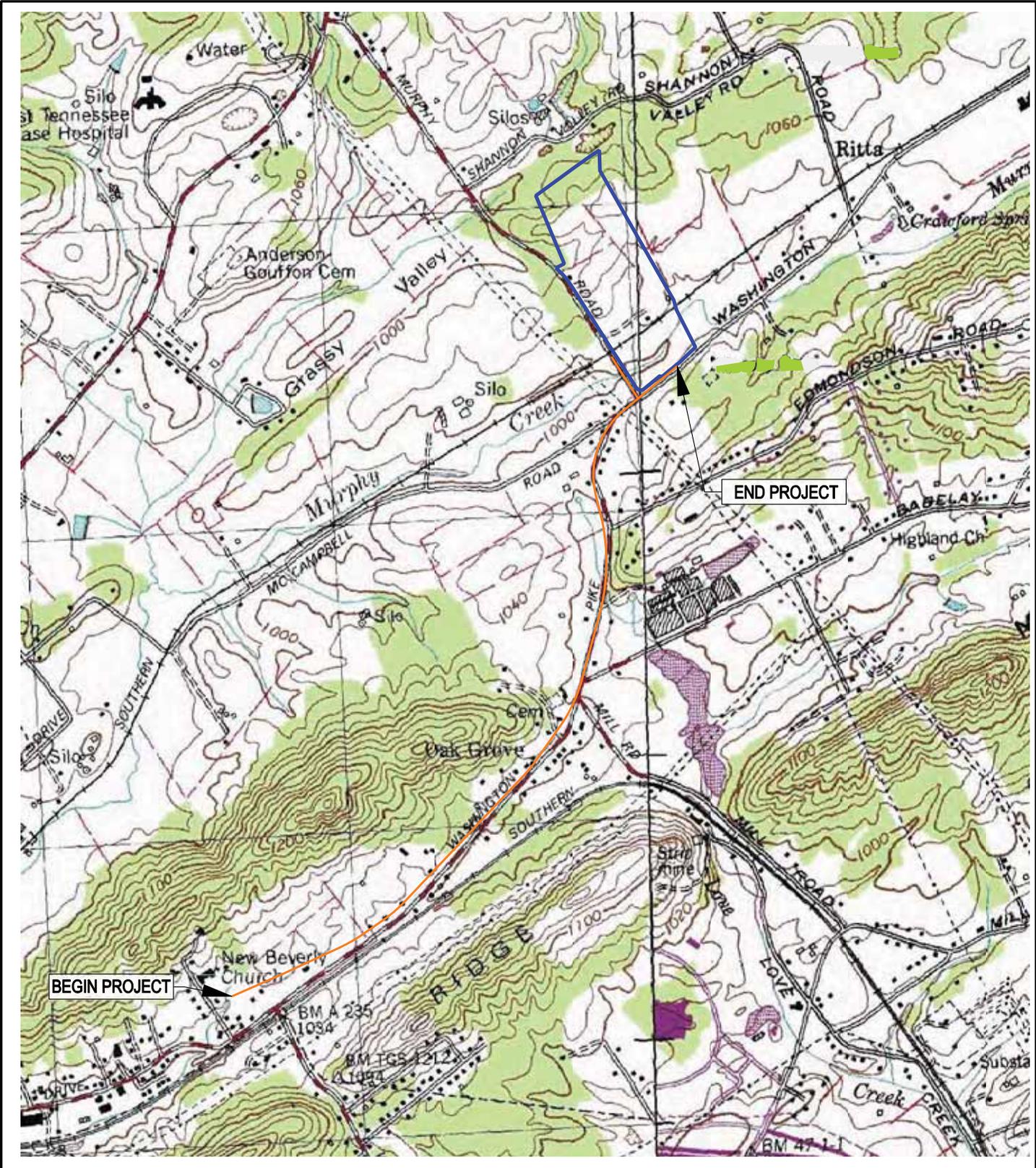
KN-13230
5835 McCampbell Drive

Situated on the north side of McCampbell Drive and oriented to the east, this is a one-story house constructed c. 1930 and rests on a wood pier foundation with concrete block infill. The house is sheathed in weatherboard and there is a brick chimney in the interior of the gable roof. The roof is standing seam metal and has exposed rafters. The windows are 6/6 sash. On the west elevation is a shed roof wing with partially-enclosed porch. The porch has square wood posts and railings. The porch wraps around the enclosed portion from the southeast to the northeast. The enclosed portion has a row of three 6/6 windows. There are two entry doors from leading this porch that are paneled. The house is covered in vegetation.



Figure 21 – KN-13230, south elevation

The house is abandoned and bank-owned. Under Criteria A and B, this house is not associated with a significant person or event, and under Criteria C it does not possess significant architectural features and has lost some of its architectural integrity due to deterioration; therefore, KN-13230 is recommended not eligible for the National Register.



	LEGEND	USGS Quadrangle Maps 7.5 Minute Series Fountain City (1978) & John Sevier (1978) (Photorevised 1992)
	PROPOSED ALIGNMENT PROPOSED NATIONAL REGISTER BOUNDARY	

FIGURE 22 -- PROPOSED NATIONAL REGISTER BOUNDARY FOR MURPHY SPRINGS FARM (KN-2586)

KN-2586
Murphy Springs Farm
4508 Murphy Road

This farmstead has been determined eligible to the National Register by the Tennessee Historical Commission (THC). The owner, Kevin Murphy, is currently preparing a nomination form for listing on the National Register. The property is also a Tennessee Century Farm (see Appendix B for application form).

The farm is located on the east side of Murphy Road at the corner with Washington Pike along Murphy Creek in an area of Knox County known as Grassy Valley. The property includes a c. 1841 Gothic Revival house and a collection of outbuildings. The Norfolk Southern Railroad, constructed in 1887 as Powell's Valley Railroad, runs through the property alongside Murphy Creek. The farm was originally purchased in 1797 by Robert Murphy and reached 192 acres by 1826. His son Hugh built the current house and purchased additional acreage. The farm at times had fields for crops of corn, potatoes, flax, and cotton and later a dairy was established. Currently the farm is in timber, fallow fields, hay and grazing fields, and a seven-acre field that is plowed by the East Tennessee Draft Horse and Mule Owner's Association.



Figure 23 – Hugh Murphy House, west elevation

Description of Buildings

Hugh Murphy House – Oriented west toward Murphy Road, the two-story frame, Gothic Revival house has a steeply-pitched side gable, standing seam metal roof. The house is sheathed in weatherboard. A cross gable in the front façade has a tripartite window with two 8-pane windows and a three-paned stained glass sidelight to either side. Above are pointed arched louvers with a medallion attic vent above. The paneled entry door has three-light sidelights to either side and a molding surround. A slightly-pedimented molding surrounds each window on

the house which are 6/6 and have storm windows. The hipped roof porch is partial-width and features square wood columns and a wood floor on brick piers. On the north elevation is a wraparound porch that extends the length of the rear cross gable and wraps to the east elevation. The hipped roof porch is supported by wood Doric columns and has a wood floor and paneled entry door with the same surround and sidelights as the front entry door. In the cross gable are two, steeply-pitched gabled dormers. A triangular louver is above the windows and also above the second floor window of the side gable which also has a medallion attic vent. On the rear or east elevation the southeastern portion of the porch is enclosed and wraps around to the south elevation. It has an entry door from the porch, a fixed three-light window, paired 3/3 window, and a 6/6 sash window on the south elevation. There is a window in the second floor of the cross gable on the east elevation in the same configuration as the north elevation side gable window. The south elevation has a paired gable set slightly back from the main side gable. The windows on the second floor of this gable are 6/6 but smaller than the rest of the windows and there is a medallion attic vent above. There is a bricked cellar entrance at the bottom of this gable. The eaves of the house are open with enclosed rafters and a wide band of trim below. There is a corbelled brick chimney with metal cap on the ridge of the side gable and also on the ridge of the cross gable that extends to the rear.



Figure 24 – Hugh Murphy house, southwest elevation

A c. 1925 renovation introduced a Craftsman porch on the front façade that consisted of a shed roof and tapered columns on brick piers. Also, in 1925, bathrooms were added in the paired gable and rooms such as a mud room and nook were added to the kitchen on the first floor to form the enclosed porch. A wall separating the central hall from the living room was removed, and a fireplace was removed from the living room. When the rear porch was enclosed, an outside entry door and molding were removed and added to the corn crib in Garage 1.

The current owner has been restoring the house to its original form in the past few years. On the

advice of the THC, the front porch was rebuilt to its original configuration according to photographs. Recent renovations to the rear wraparound porch have included removal of c. 1980 plate glass that enclosed the wraparound porch, however the three foot extension of the porch made in the c. 1925 renovation was maintained; reconstruction of the kitchen within the same footprint; replacement of the door on the east elevation into the c. 1925 enclosed porch with double windows; and removal of a gable roof from the hipped roof over the enclosed porch on the south elevation. Other renovations included replacing the cross gable chimney and fireplace, replastering the interior, new cellar entrance, and renovating the kitchen and bathrooms.



Figure 25 – Hugh Murphy house, northeast elevation

The interior of the house retains the original woodwork, stairs and railing, doors, window sills, baseboards, some of the plastering, and pine flooring. The layout is the same with the exception of a recently added downstairs bathroom and laundry room.

Garage 1 – This is a two-bay garage with corn crib in the center, built c. 1925 that rests on a concrete block foundation. Entrance to the corn crib on the west elevation is a door and molding that is from the house. The garage has weatherboard siding and a standing seam metal, gable roof.

Garage 2 – North of Garage 1 is a concrete block, one-bay garage constructed c. 1950. It has a wooden roll door with a row of glass panes at the top and a side entrance that has been boarded up with weatherboard. The gable roof has standing seam metal and there are weatherboards in the gable.

Springhouse – Constructed c. 1905 in support of the dairy operation at the farm, the gable-roofed springhouse is constructed of vertical boards and has a standing seam metal roof. It has a concrete floor and concrete block foundation that is c. 1970. The entrance is on the east elevation with a pent roof above the door. The windows are fixed with six lights. In the northeast corner inside is a

cement water trough that catches the spring water flowing into this corner of the building. There is a brick chimney and fireplace south of the springhouse. The wash house surrounding the fireplace was recently torn down. The wash house was probably used to sanitize dairy equipment. The brick piers of this structure also remain.



Figure 26 – Garage 1 and 2



Figure 27 – Springhouse and chimney for wash house

Smokehouse – This smokehouse was constructed at the same time as the house as dated by core sampling. The logs are V-notched and there is a small vertical board door in the west elevation. The roof overhangs in front of the door and there are vertical boards in the gable. The

Smokehouse is currently undergoing renovations including reconstruction of the roof with shakes, construction of a rock foundation, and replacement of a few of the sills and lower logs.

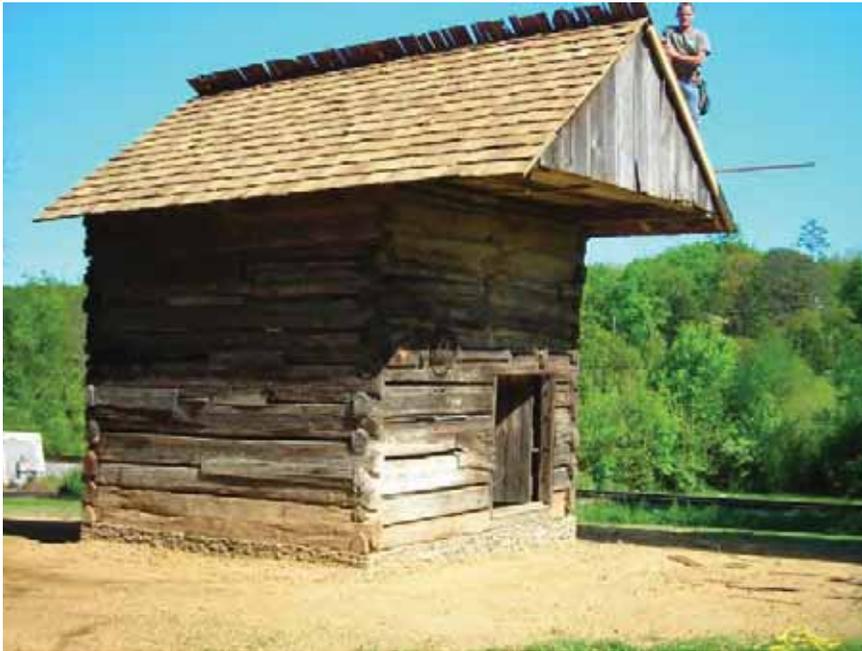


Figure 28 – Smokehouse

Wood Shed – This gable-roof structure is constructed of vertical boards with unhewn corner posts and has a standing seam metal roof. It is open on the south elevation and there is a four-pane window on the west elevation. A small shed is attached to the northeast corner. It was originally the wood shed and was moved to its current position at the end of the driveway in the 1930s.



Figure 29 – Wood Shed

Chicken Coop – This shed-roofed structure c. 1900 with standing seam metal roof was originally a chicken coop with an entry door on the west elevation. The south elevation was opened up c. 1970 and the shed is now used for storage. It is constructed of vertical boards and rests on concrete block piers and has exposed rafters.



Figure 30 – Chicken Coop

Pole Barn – The pole barn was constructed in 2000 and is used to store farm equipment. It has one large gable-roof bay and a smaller shed bay to the east. It replaces a large c. 1925 hay barn that was severely deteriorated and recently demolished.



Figure 31 - Pole Barn and Shed

Shed – Located east of the pole barn is a vertical board shed with standing seam metal roof with open bays on the south elevation.

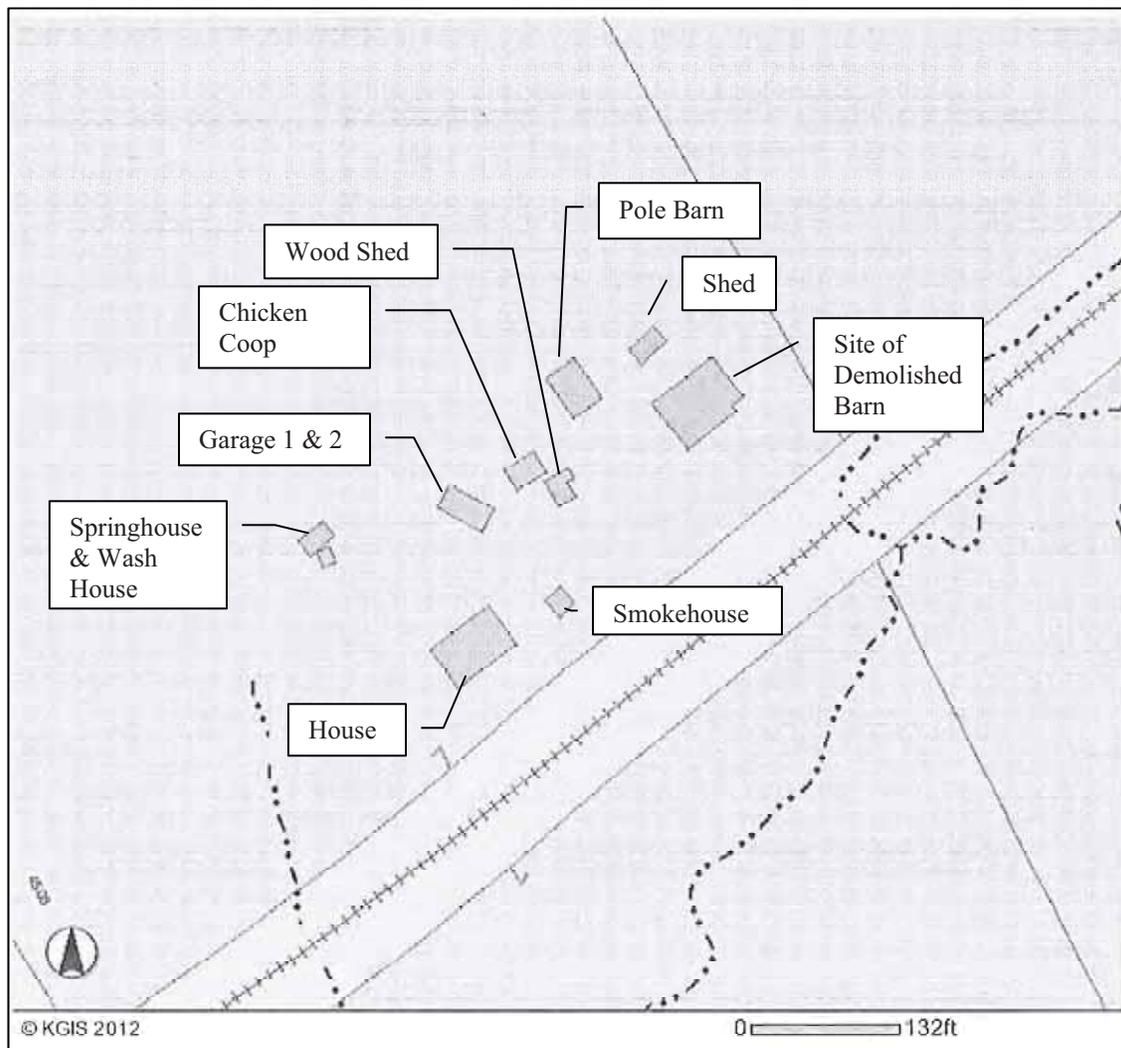


Figure 32 – Site Plan for Murphy Springs Farm complex

Eligibility for the National Register

KN-2586 is eligible for the National Register based on Criteria A as an example of a family farmstead in the Early Settlement of Knox County and Criteria C for its example of Gothic Revival architecture. The farm was purchased in 1797, less than a decade after the city of Knoxville was laid out. The acreage has been maintained as farmland or timberland and stayed within the family since that time. The farmstead, surrounded by agricultural fields, retains many buildings from the turn of the century dairy operation. The house has been restored to its original Gothic Revival appearance with characteristic steep gables, entry porch, and molding around the windows and doors.

The following buildings are contributing to the Murphy Springs Farm:

- Hugh Murphy House
- Springhouse
- Smokehouse

Wood Shed
Garage 1
Chicken Coop
Shed
Agricultural landscape of fields and timberlands

Non-contributing features include:

Garage 2
Pole Barn

Boundary of Eligible Property

The proposed boundary consists of the current parcel on which the Hugh Murphy house and its associated outbuildings sits. The parcel, 049 080, is approximately 49.50 acres with Murphy Road as its western boundary, Washington Pike as its southern boundary, parcel 049 077 forming its eastern boundary and various parcels along its northwestern and northern boundary (see Figure 32). The current owner, Kevin Murphy, had previously proposed the boundary be based on lands acquired by the original owner, Robert Murphy, which would total 207.92 acres and encompass various adjoining parcels now owned by family members. Prior submittal of this report to the TDOT resulted in a recommendation that the boundary reflect only the parcel containing the Murphy Springs Farm house and outbuildings that were associated with farming activities through the dairying period, approximately the 1920s. This would put the period of significance for Murphy Springs Farm to be from 1841, the construction of the Hugh Murphy house, to the 1920s, which marked the end of continuous farming activity. After dairying activities ended, the farm was further subdivided among family member who began their own homes and farms.



Figure 33 – Proposed National Register Boundary in blue for Murphy Springs Farm (KN-2586) with neighboring parcels shown in pink.

Assessment of Impacts under Section 106

In accordance with 36 CFR 800.5, the Criteria of Effect was applied to the proposed project improvements at the Murphy Springs Farm. Proposed improvements at the corner of Murphy Road and Washington Pike are to widen the roadway for the addition of travel and turn lanes and the installation of bike lanes, sidewalks, curb and gutter. Approximately 150 square feet for a temporary construction easement will be required along Murphy Road from within the proposed National Register boundary. Approximately 310 square feet will be needed for temporary construction easement along Washington Pike from within the proposed National Register boundary (see Figure 33).

Murphy Road currently widens from 24 feet at the railroad to 36 feet at the intersection with Washington Pike to accommodate a right-turn lane. After proposed improvements are completed, the width of Murphy Road would be 44 feet at the railroad and 55 feet at the intersection in order to accommodate southbound dedicated right and left turn lanes and a northbound second travel lane that merges into one lane at the railroad. Retaining walls would be required where the roadway intersects Murphy Creek. These walls would be three to five feet in height and extend for 313 feet along the west side of the roadway and 200 feet along the east side. The material and aesthetics of the retaining wall would be determined during the design process after reviewing comments from the public received during the public hearing. Retaining walls were chosen for this area as opposed to a roadway embankment in order to avoid permanently impacting the proposed National Register boundary for the Murphy Springs Farm.

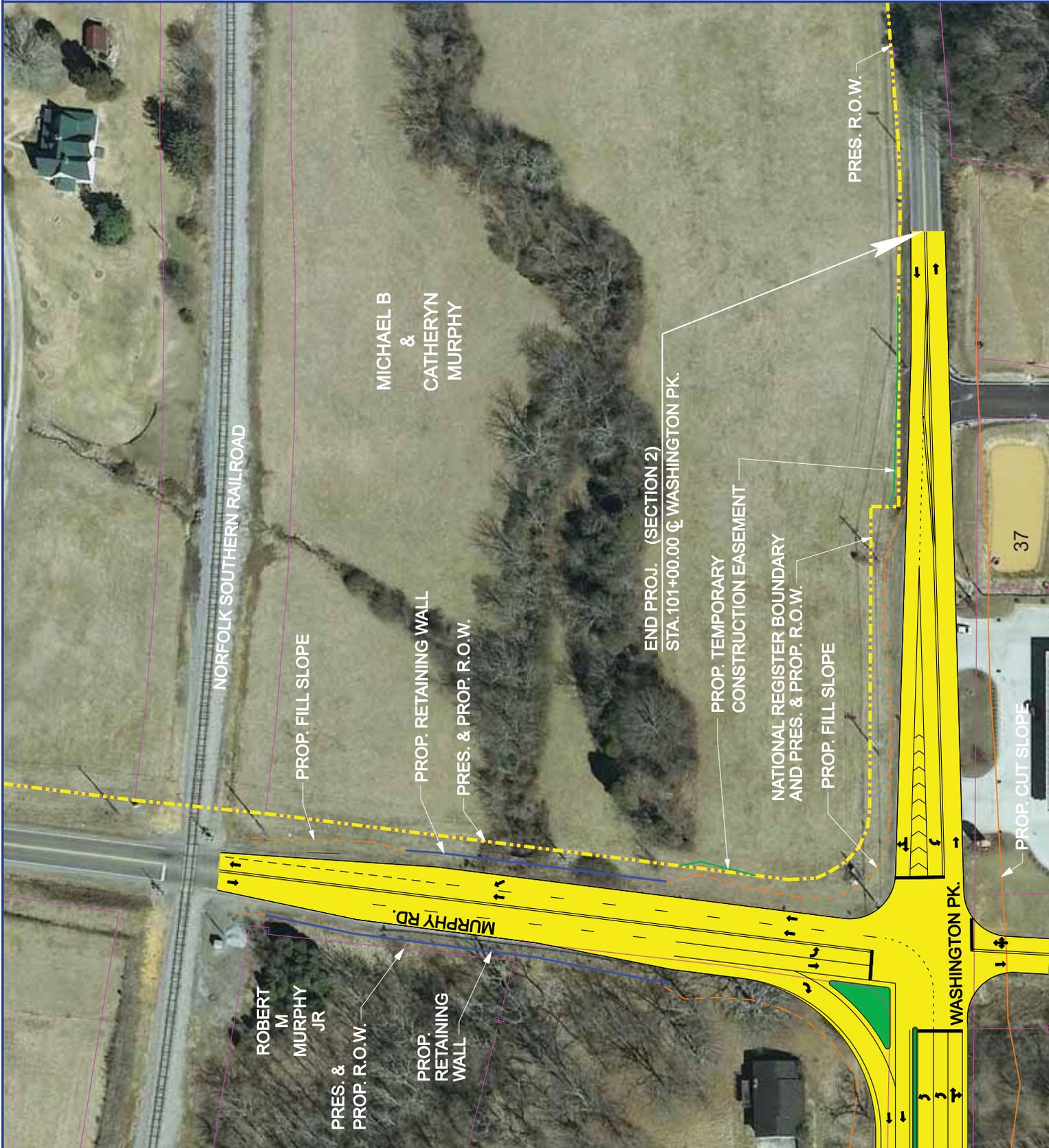
Washington Pike currently widens from 24 feet to 34 feet as it approaches the intersection with Murphy Road. After proposed improvements are completed, the width of Washington Pike would be 70 feet in order to accommodate a dedicated left turn lane separated by a median and sidewalks.

Only temporary construction easements would be necessary for the proposed improvements along Murphy Road and Washington Pike. No right-of-way is required from within the proposed National Register boundary for the proposed improvements. The proposed project would not cause the physical destruction or removal of any structure. The proposed easements contain grassy fields that are mowed for hay along Murphy Road and Washington Pike and once the proposed project is completed, the easement would be returned to grass. The proposed project will not change the property's function as agricultural fields or its setting in a rural environment that has some urban incursions.

With the proposed project's improvements of roadway widening and retaining walls, no visual, atmospheric or audible elements would be introduced that would diminish the National Register significance of the farm and its buildings. The grassy fields of the farm currently front a busy intersection that is signalized and has utilities and commercial businesses at the corner. The addition of turn lanes along Murphy Road will alleviate some queing of traffic in front of the farm. The traffic currently ques northward to beyond the railroad during peak traffic hours. Traffic patterns would not be changed due to the proposed project. No changes in access to the property are anticipated.

Figure 34
Roadway Improvements
at Murphy Springs Farm
(KN 2586)

LEGEND	
	Proposed Improvements
	National Register Boundary
	Proposed Fill Slope
	Proposed Cut Slope
	Proposed Retaining Wall
	Proposed Temporary Construction Easement
	Present R.O.W.



The Hugh Murphy House and outbuildings are within view of the proposed project along Murphy Road near the railroad crossing. The house is approximately 530 feet from the proposed project's endpoint along Murphy Road at the railroad and approximately 580 feet from edge of right-of-way where the tree line along Murphy Creek intersects with Murphy Road. At this location the proposed improvements include fill, retaining walls in place of an existing guard rail, and widening of the roadway within right-of-way. The tree line then blocks the viewshed of the rest of Murphy Road (see Figure 34). There are no buildings within view of the proposed project along Washington Pike due to the tree line along Murphy Creek blocking the viewshed toward the roadway (see Figure 35). Proposed improvements along Washington Pike east of the intersection with Murphy Road include widening and fill within the right-of-way. Therefore, no impacts to the viewshed and setting of the historical property are anticipated that would diminish the qualities that make this resource eligible for the National Register.

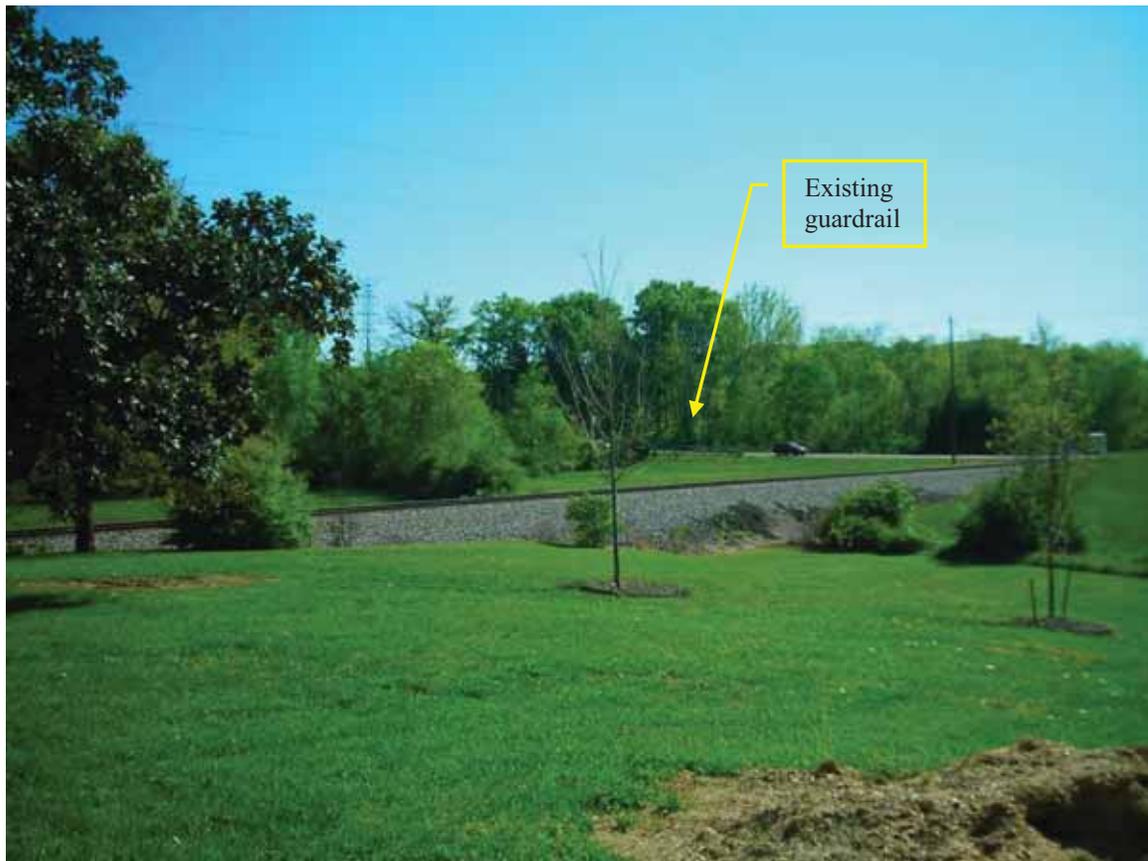


Figure 34 – View from front façade of the Hugh Murphy House southwest across railroad tracks toward Murphy Road.



Figure 35 – View from the east elevation of the house south toward Washington Pike.

A noise study was conducted to assist in evaluating the potential for noise impacts to the Murphy Springs Farm. This study is on file with TDOT. The study found that the predicted noise level for 2012 (existing) at the Weigel’s convenience store which is located at the southeast corner of Washington Pike and Murphy Road directly across from the Murphy Springs Farm is 63 dBA (a unit of noise measurement). If no actions are taken to improve the roadway, then the noise level will increase to 65 dBA by design year 2033 (future). If the proposed improvements are implemented then the noise level will remain at 63 dBA by 2033. The study also modeled a point in the field located on the west side of Murphy Road (Parcel 049 083) across from the Murphy Springs Farm. The point is located approximately 700 feet from the project endpoint at the railroad on Murphy Road. The existing noise level is 46 dBA, the future level is 48 dBA with no action and 48 dBA with proposed improvements (see Table 1).

Table 1 – Results of Noise Study at Murphy Springs Farm			
Location	Existing (2012)	Future (2033) with No Action	Future (2033) with Improvements
Weigels’ at Washington Pike and Murphy Road	63 dBA	65 dBA	63 dBA
Parcel 049 083 west of Murphy Road	46 dBA	48 dBA	48 dBA

FHWA developed a Noise Abatement Criteria (NAC) based on land uses establishing base lines for various activities to determine when the level of impact from traffic noise occurs. The Murphy Springs Farm is considered a residential land use and therefore falls into Category B which has a baseline dBA of 67 (see Table 2).

Table 2 – FHWA Noise Abatement Criteria

Activity Category	dBA	Location	Description of Activity
A	57	Exterior	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	67	Exterior	Residential
C	67	Exterior	Active sport areas, amphitheatres, auditoriums, campgrounds, cemeteries, day care centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreational areas, Section 4(f) sites, schools, television studios, trails, and trail crossings.
D	52	Interior	Auditoriums, day care centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
E	72	Exterior	Hotels, motels, offices, restaurants/bars, and other developed lands, properties or activities not included in A-D or F.
F	NA	NA	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, ship yards, utilities (water resources, water treatment, electrical), and warehousing.
G	NA	NA	Undeveloped lands that are not permitted for development.

Source: FHWA Noise Policy FAQs

An increase in noise is considered by TDOT to be “substantial” when the dBA increases 10 to 15 dBA. Noise levels in the area of Murphy Springs Farm are anticipated to increase by two dBA with or without roadway improvements by design year 2033. While noise levels may increase at the Murphy Springs Farm, the level of noise is not considered to be an impact according to the FHWA’s Noise Abatement Criteria or TDOT’s criterion of substantial increase. Therefore, the overall environment of the Murphy Spring Farm would not be diminished due to noise levels from the project.

It is the opinion of the consultant that the proposed project would not have an adverse effect to the Murphy Springs Farm.

Assessment of Impacts under Section 4(f)

The proposed project would require temporary construction easement from the property which does not constitute a “use” under Section 4(f) (23 CFR 771.135 (p)(7)). It is the opinion of the consultant that the proposed project would not have an adverse effect to the Murphy Springs Farm; therefore, there will not be a Section 4(f) use of the historic property.

Conclusion

CDM Smith conducted the historic structures survey for improvements Washington Pike. The project is located in the City of Knoxville in Knox County with its western terminus at I-640 and its eastern terminus at Murphy Road. This area was previously surveyed in 1984 and 2000 for resources eligible to the NRHP. Within the APE are 13 resources determined not eligible and one resource determined eligible for the NRHP. Murphy Springs Farm (KN-2586) is eligible for the NRHP based on Criteria A as an example of a family farmstead in the Early Settlement of Knox County and Criteria C for its example of Gothic Revival architecture. The eligible property was examined for potential effects by the project. It is the opinion of the consultant that the proposed project would have no adverse effect to the eligible property and, therefore, there would be no Section 4(f) use of a historic property.

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- 2009 Swiss Settlers, Knoxville. *The Tennessee Encyclopedia of History and Culture*. Available online at <http://tennesseeencyclopedia.net/entry.php?rec=1288>.

Bennett, Ann

- 1994 "Historic and Architectural Resources in Knoxville and Knox County, Tennessee." National Register Nomination Form, Knoxville-Knox County Metropolitan Planning Commission, Knoxville, TN.

Brown, Nissa Dahlin

- 1980 "Mechanicsville Historic District." National Register Nomination Form, East Tennessee Development District, Knoxville, TN.

Knoxville-Knox County Metropolitan Planning Commission

- 2007 "North City Sector Plan." The Knoxville-Knox County Metropolitan Planning Commission, Knoxville, TN.

Middle Tennessee State University

- 2000 *Powerful Artifacts: A Guide to Surveying and Documenting Rural African-American Churches in the South*. Center for Historic Preservation, Middle Tennessee State University, Murfreesboro, TN.

Rule, William, ed.

- 1900 *Standard History of Knoxville, Tennessee*. Lewis Publishing Company, Chicago, IL.

Sammartino, Therese T.

- 1996 "Knoxville National Cemetery." National Register Nomination Form, East Tennessee Development District, Knoxville, TN.

Thomason and Associates

- 2000 *Historical and Architectural Survey. Improvement to Tazewell Pike (SR 331) and Washington and Millertown Pikes Knoxville, Knox County, TN*. Thomason and Associates, Nashville, TN.

United States Geological Survey

- 1941 *7.5' Fountain City, Tennessee Quadrangle*. Department of the Interior, Geologic Survey, Washington, D.C.; available online at <http://www.lib.utk.edu/cic/tnmap/browse.html>

- 1953 *7.5' Fountain City, Tennessee Quadrangle*. Department of the Interior, Geologic Survey, Washington, D.C.; available online at <http://www.lib.utk.edu/cic/tnmap/browse.html>

- 1966 *7.5' Fountain City, Tennessee Quadrangle*. Department of the Interior, Geologic Survey, Washington, D.C.; available online at <http://www.lib.utk.edu/cic/tnmap/browse.html>

- 1978 7.5' *Fountain City, Tennessee Quadrangle*. Department of the Interior, Geologic Survey, Washington, D.C.; available online at <http://www.lib.utk.edu/cic/tnmap/browse.html>
- 1978 7.5' *John Sevier, Tennessee Quadrangle*. Department of the Interior, Geologic Survey, Washington, D.C.; photo revised in 1992.

Appendix A
Resume for Principal Investigator

Jana L. Bean

Architectural Historian

Ms. Bean joined the Columbia, South Carolina office in 2003 and serves as an architectural historian and environmental coordinator. In these roles, she investigates and documents survey findings; supervises historic survey field personnel; completes NEPA documentation; conducts environmental assessments for transportation projects; and assesses the impacts related to human environmental conditions.

Principal Investigator, S-41 Blackstock Road over Norfolk Southern Railroad, Spartanburg County, South Carolina (2011-2012). This project involved an investigation and presentation of 3 alternative layouts for consideration by SCDOT for a new grade-separated bridge to carry S-41 (Blackstock Road) over the Norfolk Southern Railroad to replace existing at-grade crossing. Ms. Bean conducted field and written documentation of environmental conditions and assessed the impacts of the project. As the principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Principal Investigator, South Carolina Public Railways, Charleston Intermodal Facility, North Charleston, South Carolina (2011). As the principal investigator, Ms. Bean reviewed historic maps, performed background research on previous surveys, documented existing conditions at the former Charleston Naval Base through photography, surveyed additional properties and made recommendations of eligibility for the National Register, and determined the potential impacts of the project on eligible and listed historic sites and districts.

Principal Investigator, North Forrest Street (CR 138) Improvements, Georgia Department of Transportation, Valdosta, Georgia (2010). Ms. Bean performed an assessment of impacts to 2 historic districts and 3 historic properties. This process involved documentation in an Assessment of Effects report and Draft 4(f) Evaluation, analysis of avoidance alternatives, and negotiation of mitigation measures.

Principal Investigator, Donalsonville-Seminole County Airport Environmental Assessment, Donalsonville, Georgia (2010-2011). As the principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Environmental Coordinator, I-526 (Mark Clark Expressway) around Charleston, South Carolina (2008-2010). The extension of I-526 (Mark Clark Expressway) around Charleston, South Carolina will complete the Charleston Inner Belt Freeway and provide an interstate connection with West Ashley, Johns Island, and James Island. CDM Smith was selected by the South Carolina Department of Transportation to study alternative alignments, prepare an environmental impact statement, and produce right-of-way plans for this new interstate facility. For the environmental impact statement portion of this study, Ms. Bean was responsible for documenting environmental conditions, assessing impacts to Section 4(f) resources, and coordinating with local and state officials. She also

Education

M.A. - History,
University of
South Carolina,
2003

B.A. - History,
Southwest
Missouri State
University, 1996

Training

National
Preservation
Institute -
Identification and
Evaluation of Mid-
20th Century
Buildings

National
Preservation
Institute - Section
106 Review

Ohio Department
of Transportation -
Section
106/National
Register Eligibility
Training

Ohio Department
of Transportation
Section 4 (f)
Training

National Highway
Institute - NEPA
and the
Transportation
Decision-Making
Process

Years of Experience

Total Years: 8
CDM Smith: 8

served as the principal investigator for a historic structures survey involving documentation of historic sites, making recommendations of eligibility to the National Register and potential project impacts, and coordinating mitigation with the SHPO.

Principal Investigator, Hilton Head Island Airport - Environmental Assessment for Runway 21 Tree Removal, Hilton Head, South Carolina (2008-2012). This airport's primary approach contained hundreds of obstructions. The controversial environmental assessment focused on a strict tree removal ordinance as well as nearby historic properties, endangered species, and residential impacts. As the principal investigator for a historic architectural survey, Ms. Bean assessed determinations of eligibility to the National Register for historic structures, the eligibility of a Traditional Cultural Property, and the potential effect of the project on and possible mitigation for a Civil War earthwork and archaeological site.

Principal Investigator, Turner County Airport Runway Expansion Environmental Assessment, Ashburn, Georgia (2008-2009). As the principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Principal Investigator, State Route 9 Environmental Assessment, Lee and Union Counties, Mississippi (2008). As the principal investigator for this proposed widening and new alignment for SR 9, Ms. Bean developed a historic context, documented historic site survey. She conducted mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Principal Investigator, Armuchee Connector, Floyd County, Georgia (2007). This project consisted of a 2-mile long, 2-lane rural and 4-lane urban connector with bike lanes. Also included was the design of a 2-lane bridge with a multi-use trail over the Oostanaula River. As principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Principal Investigator, Roadway and Bridge Design for Widening of I-26, North Charleston, South Carolina (2006-2007). For the South Carolina Department of Transportation, this project involved preliminary design, construction cost estimates, and an environmental assessment for the widening of a 6-mile section of I-26 between I-526 and Exit 217. It was completed on an accelerated 6-month schedule. As the principal investigator, Ms. Bean developed a historic context for World War II housing developments; field documentation of historic sites which included mapping, photography, and completion of SHPO survey forms; and recommendations of eligibility for the National Register and determinations of the effect of the project to historic resources.

Environmental Coordinator, I-73 Environmental Impact Assessment, South Carolina (2005-2007). CDM Smith worked with SCDOT on the planning and design of I-73, to run from Michigan to South Carolina. The team prepared two environmental impact statements and records of decision and obtained environmental permits within an unprecedented three-year period. For this project, Ms. Bean coordinated with the cultural resource firm subconsultant, documented environmental conditions, assessed impacts to Section 4(f) resources, coordinated with local and state officials, assisted at public meetings, and conducted public surveys.

Environmental Coordinator, Mississippi River Bridge Crossing Feasibility and Location Study, Memphis, Tennessee 2004-2007). This feasibility and location study, which included three counties in three states, identified and evaluated potential river crossing sites. CDM Smith conducted extensive public and community outreach as part of a context sensitive solutions program. The project received an ACEC award. Ms. Bean oversaw mapping and data collection of the environmental conditions, coordinated with public agencies and the general public, and developed the purpose and need for the project.

Principal Investigator, Southern Kentucky Intermodal Park Environmental Assessment, Somerset, Kentucky (2004). Environmental services were provided for the proposed intermodal park study for the Southern Kentucky Economic Development Corporation. A community impact report and a noise analysis report were prepared. A categorical exclusion, Level III report was submitted to and approved by KYTC and FHWA. As the principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Principal Investigator, North Campbell Station Road Advanced Planning Report and Environmental Assessment, Farragut, Tennessee (2003-2005). CDM Smith determined the need and feasibility of improving access to I-40 via Campbell Station Road. The study recommended widening the roadway to 5 lanes throughout, and evaluated the need for a new traffic signal and signal system upgrades. As the principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Principal Investigator, Proposed Route I-66 between Somerset to London, Section 106 Compliance, Kentucky (2001-2007). As the principal investigator, Ms. Bean developed a historic context, documented historic sites through mapping, photography, completed SHPO survey forms, and made recommendations of eligibility for the National Register and determinations of effect.

Environmental Coordinator, College Avenue (SC 133) Widening and Railroad Bridge Replacement, Clemson, South Carolina (2001-2010). For this project CDM Smith provided surveys and mapping, traffic analysis, environmental studies, evaluation of alternatives, coordination with Norfolk Southern Railroad and utility companies, temporary railroad detour planning, roadway and railroad bridge construction plans, and

geotechnical engineering for pavement and bridge foundation design. As the environmental coordinator, Ms. Bean conducted field and written documentation of environmental conditions and assessed the impacts of the project.

Environmental Coordinator, Veteran's Administration Enhanced-Use Market and Business Plan, Columbia, South Carolina (2005-2011). This study primarily included data collection, defining Veteran's Administration development objectives, refining preferred development objectives, real estate appraisal and market analysis for the future of the enhanced-use development of the VAMC campus. Ms. Bean served as the coordinator for Section 106 consultation between the Veteran's Administration and SHPO and between the Veteran's Administration and subconsultant.

Publications

Bean, J.L. "Historic Structures Report: The Slave Quarters of Redcliffe Plantation State Historic Site." July 2002

Bean, J.L. "National Register Nomination: Old Shandon Historic District." Columbia, South Carolina, December, 2002

Appendix B Coordination



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

ENVIRONMENTAL DIVISION
SUITE 900, JAMES K. POLK BUILDING
505 DEADERICK STREET
NASHVILLE, TENNESSEE 37243-1402
(615) 741-3655

JOHN C. SCHROER
COMMISSIONER

BILL HASLAM
GOVERNOR

August 17, 2012

The Cherokee Nation
17675 South Muscogee
Tahlequah, OK 74465
Attn: Dr. Richard Allen, Policy Analyst

SUBJECT: Section 106 Initial Coordination for Proposed Washington Pike Project from I-640 to Murphy Road NE, Knoxville, Knox County, Tennessee

Dear Dr. Allen:

The Tennessee Department of Transportation (TDOT) in cooperation with the Federal Highway Administration (FHWA) is proposing to improve Washington Pike from I-640 to Murphy Road NE in Knox County (maps attached). The project will widen the road from two lanes to four with turn lanes as required at intersecting side streets as well as add curb and gutter, sidewalks, and bike lanes; some sections of the road already feature a center turn lane. The intersection with McCampbell Drive near Murphy Road NE will be realigned. The project may eliminate or reduce some curb and gutter section with construction of grass swales. The approximate length of the project is 1.73 miles. Additional right-of-way will be needed.

The National Historic Preservation Act (NHPA) recognizes that federally funded undertakings, like the subject project, can affect historic properties to which your tribe attaches religious, cultural, and historic significance. In accordance with 36 CFR 800 regulations implementing compliance with Section 106 of the NHPA, I would like to know if you have information you could share with me about tribal concerns in the project area and if you wish to be a consulting party on the project? Early awareness of your concerns can serve to protect historic properties valued by your tribe.

If you act as a consulting party you will receive archaeological assessment reports and related documentation, be invited to attend project meetings with FHWA, TDOT, and the Tennessee State Historic Preservation Office (TN-SHPO), if any are held, and be asked to provide input throughout the process. If you choose to not act as a consulting party at this time, you can do so at a later date simply by notifying me.

Please respond to me via letter, telephone (615-741-5257), fax (615-741-1098), or E-mail (Gerald.Kline@tn.gov). I respectfully request responses (email is preferred) to project reports and other materials within thirty (30) days of receipt if at all possible. Thank you for your assistance.

Sincerely,

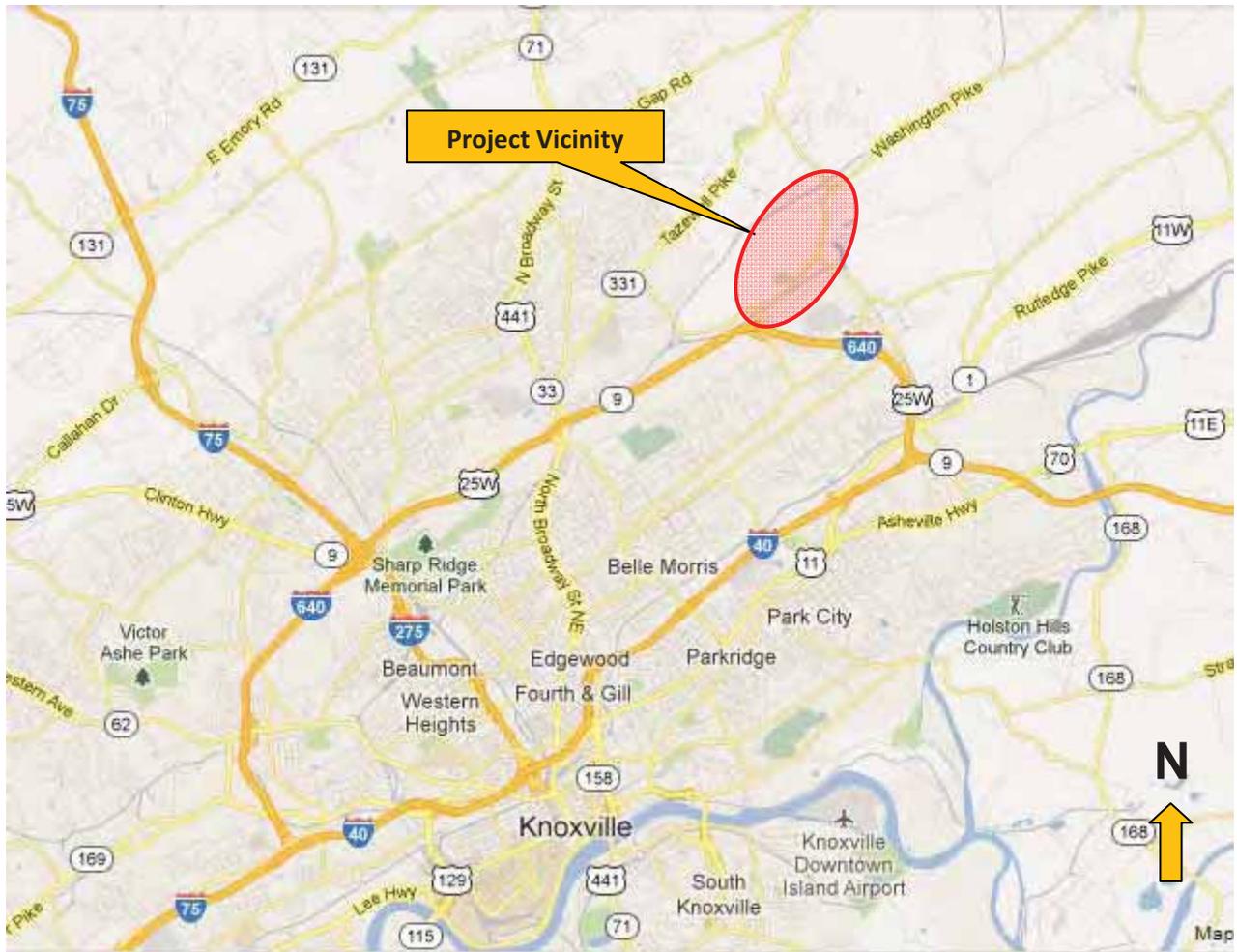
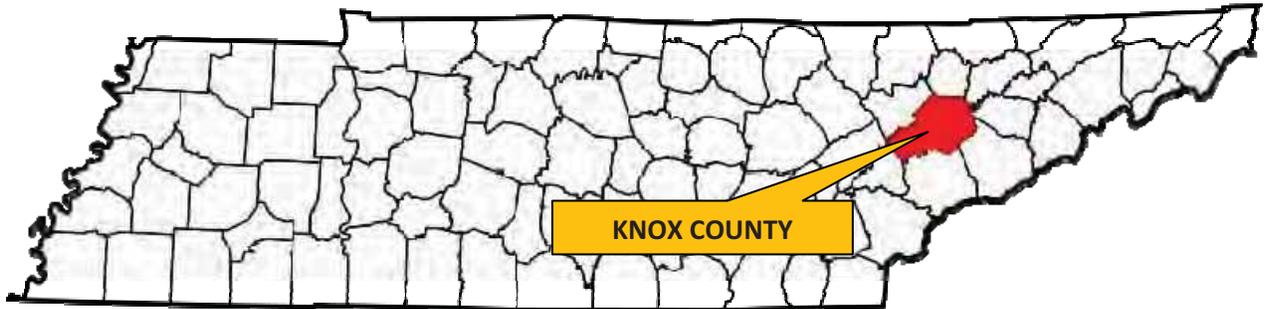
Gerald Kline
Transportation Specialist I
Archaeology Program Manager

Enclosure

cc Robin Dushane, Eastern Shawnee Tribe of Oklahoma
Kim Jumper, Shawnee Tribe
Lisa LaRue-Baker, United Keetowah Band of Cherokee Indians
Tyler Howe, Eastern Band of Cherokee Indians

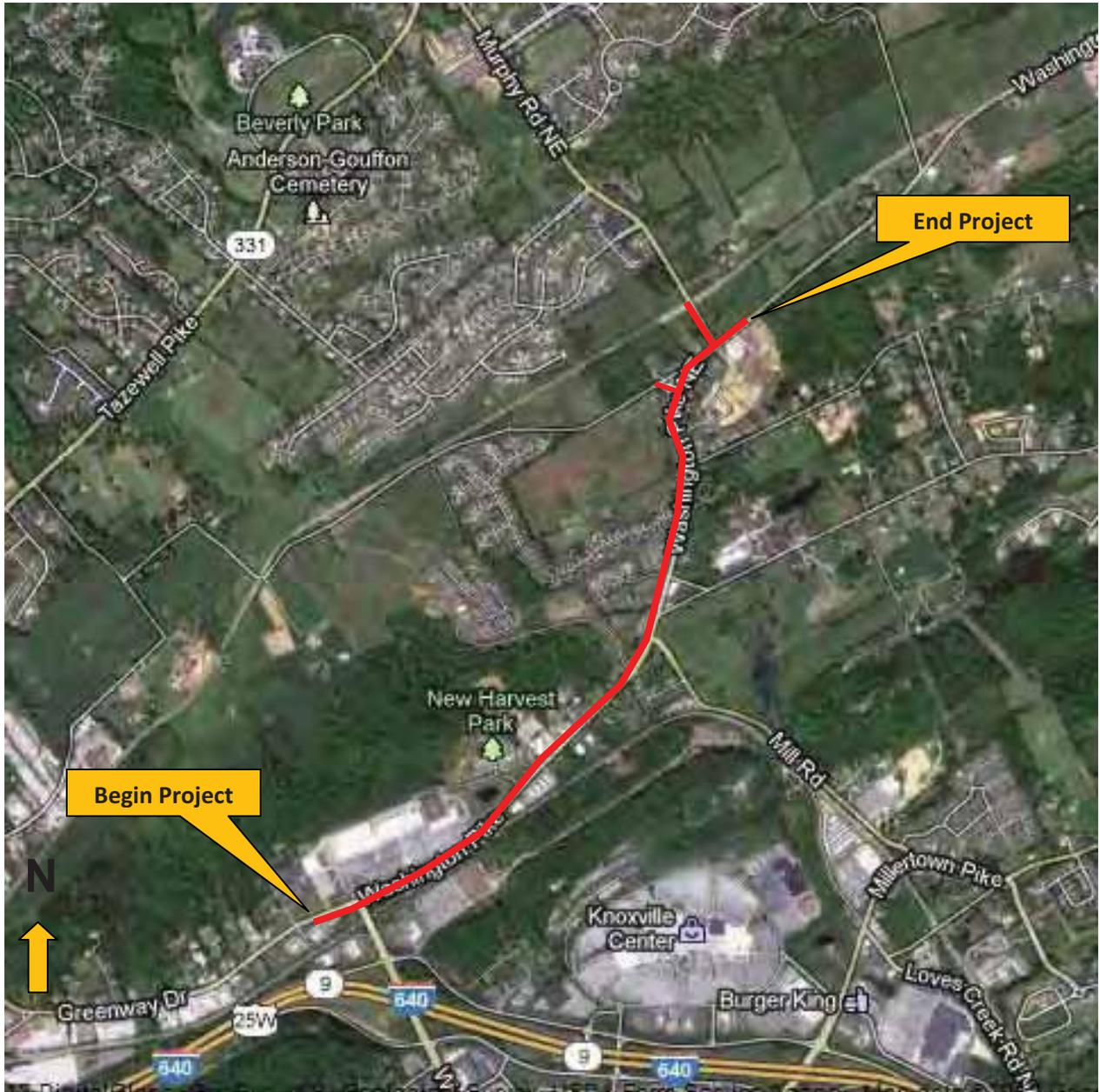


WASHINGTON PIKE PROJECT
FROM I-640 TO MURPHY ROAD NE
KNOXVILLE, KNOX COUNTY, TENNESSEE

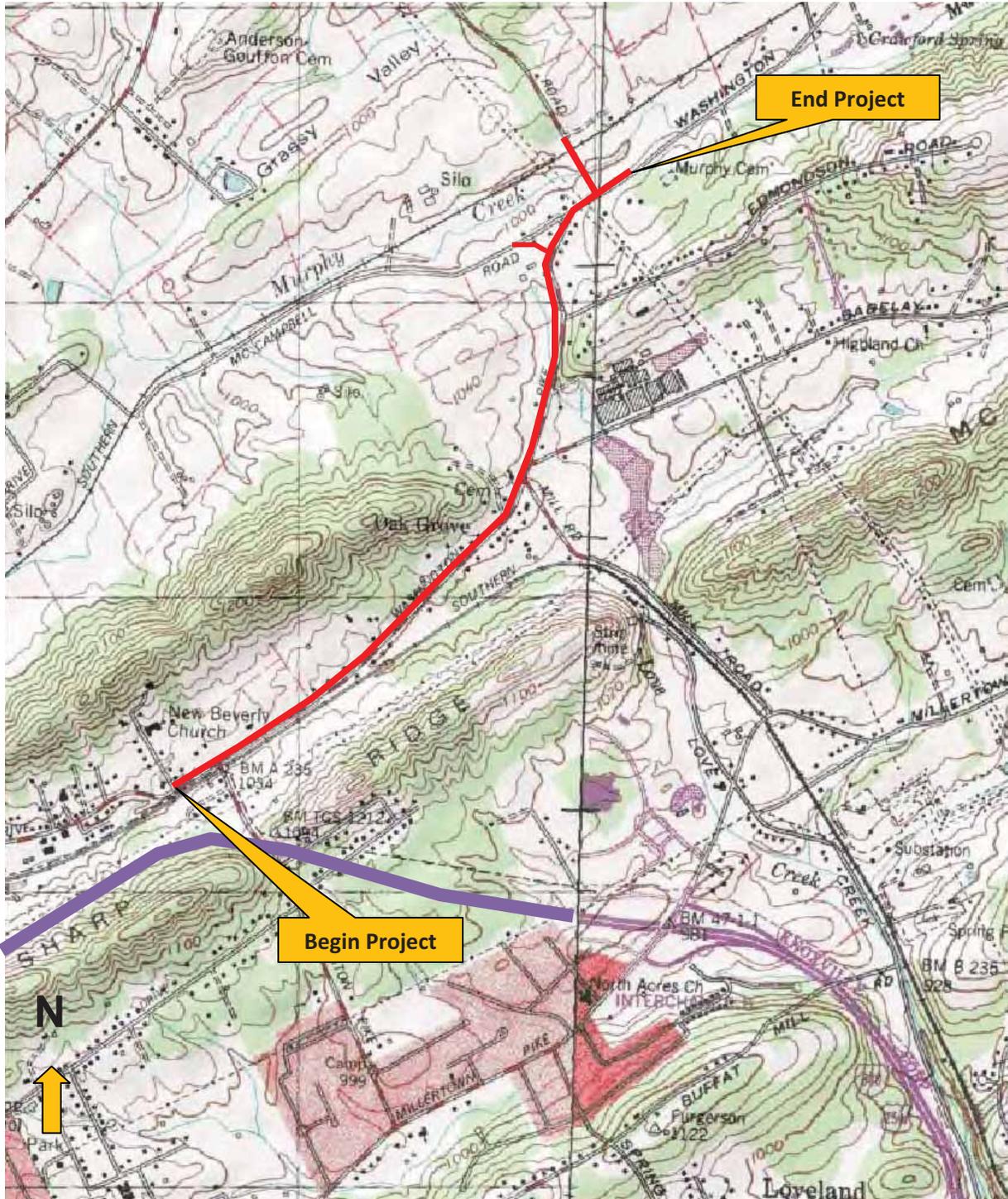


PROJECT VICINITY MAP





Project Location Map



Project Location Map – USGS Fountain City (146 SW) & John Sevier (146 SE)

List of Interested Parties

East Tennessee Community Design Center
1300 North Broadway
Knoxville, TN 37917

Heather Bailey
Historic Preservation Planner
East Tennessee Development District
Post Office Box 249
Alcoa, TN 37701-0249

East Tennessee Historical Society
P.O. Box 1629
Knoxville, TN 37901

Knox County Mayor
Suite 615, City-County Building
400 Main Street
Knoxville, TN 37902

Tennessee Valley Authority
Cultural Resources
400 West Summit Hill Drive
Knoxville, TN 37902

Knox Heritage, Inc.
P. O. Box 1242
Knoxville, TN 37901

Knoxville Historic Zoning Commission
c/o Knoxville/Knox Co. Planning Commission
City County Building, Suite 403
400 West Main Street
Knoxville, TN 37902-2476

Steve Cotham
Knox County Historian
Knox County Public Library
500 West Church Avenue
Knoxville, TN 37902-2505

Ethiel Garlington
East TN Preservation Alliance
Post Office Box 1242
Knoxville, TN 37901

Kevin Murphy
4508 Murphy Rd
Knoxville, TN 37918

Northeast Knox Preservation Association
P.O. Box 5863
Knoxville, TN 37928

Kevin P. Murphy
4508 Murphy Rd
Knoxville, TN 37918-9179
murphysprings@gmail.com
2012 April 15

RE: Washington Pike Widening

Via CERTIFIED MAIL and E-Mail

James R. Hagerman, Director of Engineering
1400 Loraine Street
Knoxville, TN 37921

Dear Mr. Hagerman,

I understand that a project is in the planning and engineering phase to widen Washington Pike from I-640 to the Murphy Road intersection. I also understand that federal funds are being used for this project.

The area of the widening project begins at a busy interstate and transits through a mix of commercial, residential and agricultural land before terminating at my family's farm. The Murphy Road / Washington Pike intersection is the gateway to northeast Knox County, which is still largely rural and agricultural in nature. Past the intersection, Washington Pike is identified as a Rural Heritage Corridor in the Northeast County Sector Plan. Also, the intersection is a sharp Growth Plan boundary line between the Urban Growth Area and Rural Area, with no transitional Planned Growth Area.

Improvements to the roadway should take into account:

- The large impact that it will have on the residences and neighborhoods
- National Register eligible structures and properties within the boundary area
- The transition from urban to rural that occurs in the 1.6 mile length of the project
- Enablement of the Washington Pike Heritage Corridor
- The generally one-way flow of high volume traffic during weekday rush-hour

First, I would like to make sure that the planners are aware of my farm's historical nature, that an impact analysis is performed as required by Section 106 since federal funds are being used, and that the impact of the project on the farm is mitigated.

The Murphy Springs Farm was settled in approximately 1797 by my ancestor Robert Murphy, and his family. His son, Hugh Murphy, built a house in 1841 that is about 850 feet from the current Washington Pike / Murphy Road intersection. That structure and its associated outbuildings have been identified as **National Register eligible** since the 1982-1986 Metropolitan Planning Commission survey of historic sites. During recent renovation and restoration, local and state historic preservation officials were consulted to ensure that the structure and farm would remain National-Register eligible.

In 2010 all of the parcels of the farm remaining in the family were certified by the Tennessee Department of Agriculture and Center for Historic Preservation at Middle Tennessee State University as a **Tennessee Century Farm**. Recently I have spoken with local preservation staff at Metropolitan Planning Commission as well as with Patrick McIntyre, the Executive Director of the Tennessee Historical Commission, and we decided to increase the scope of the National Register designation that I am preparing from just the Hugh Murphy House to the entire Murphy Family farm. I am enclosing a list of the parcels that will be listed on the National Register application, along with a rough map. I plan to submit the application to the Tennessee Historical Commission in June 2012.

Since 1797 when the Murphy's first acquired property for the farm, a number of takings have occurred that have impacted the value and historical integrity of the farm. They include:

- Early and continued use of Washington Pike, running through the center of the original farm
- Early and continued use of Murphy Road
- Railroad easement
- 200 foot TVA / KUB high voltage transmission easement on western parcels
- Water, gas and electrical utilities located adjacent to Murphy Road and Washington Pike that impact the peripheral use of the property
- Right of way acquisition for the Murphy Road widening in late 1990s

Given the historic nature of the Murphy Springs Farm and the adverse impact of prior takings, I hope and expect that all efforts will be taken to mitigate the impact to the farm, including:

- Minimal or no acquisition of farm property for right of way
- Noise mitigation measures
- Landscaping buffers
- Light pollution and trespass from streetlights and stoplights
- Location of utilities

Secondly, I hope that efforts are made to minimize the impact on other residents of the area. I have noticed that Knoxville does not utilize full-cutoff streetlights in many areas. This is a rural, residential area and full-cutoff streetlights should be a requirement.

Thirdly, Washington Pike is a route that has traditionally provided quick access for residents of the area to the interstate. There are not many stoplights. The last stoplight on Washington Pike is the light at the Murphy Road intersection; beyond that there are no lights or stop signs until the end of the road.

I have observed that Washington Pike's two-lane facility currently provides good service for most of the day, except for the morning and evening week day rush hours. At these times the traffic is generally uni-directional in nature – flowing into Knoxville in the morning and from the interstate in the evening.

Given the uni-directional nature of rush hour traffic, generally good service during non-rush-hour times, and the traditional quick transit times that Washington Pike has provided to residents, I would encourage the engineers to consider the use of high speed roundabouts instead of stop signs in the widening project. I have lived in areas of the United States and in other countries where roundabouts provide excellent service levels to travelers. In the case of Washington Pike, a multi-lane roundabout design can probably handle anticipated growth events.

I am requesting documentation on the traffic forecasting estimates that are being used as requirements in the engineering process. The Washington Pike Transportation planning Report study did not provide detailed information on the growth forecasts.

If there are any public meetings that will be held on this project, I request to be notified of them.

Sincerely,

A handwritten signature in black ink, appearing to read "K. Murphy", is enclosed in a thin black rectangular border.

Kevin P. Murphy

CC via email:

Tom Clabo, Chief Civil Engineer, City of Knoxville
Lisa Starbuck, President, Northeast Knox Preservation Association
Ronnie Collins, President, Alice Bell / Spring Hill Neighborhood Association
Nick Della Volope, 4th District, Knoxville City Council
Dave Wright, 8th District, Knox County Commission
Nathan Benditz, Knoxville Regional Transportation Planning Organization
Kaye Graybeal, Knox Metropolitan Planning Commission Historic Preservation

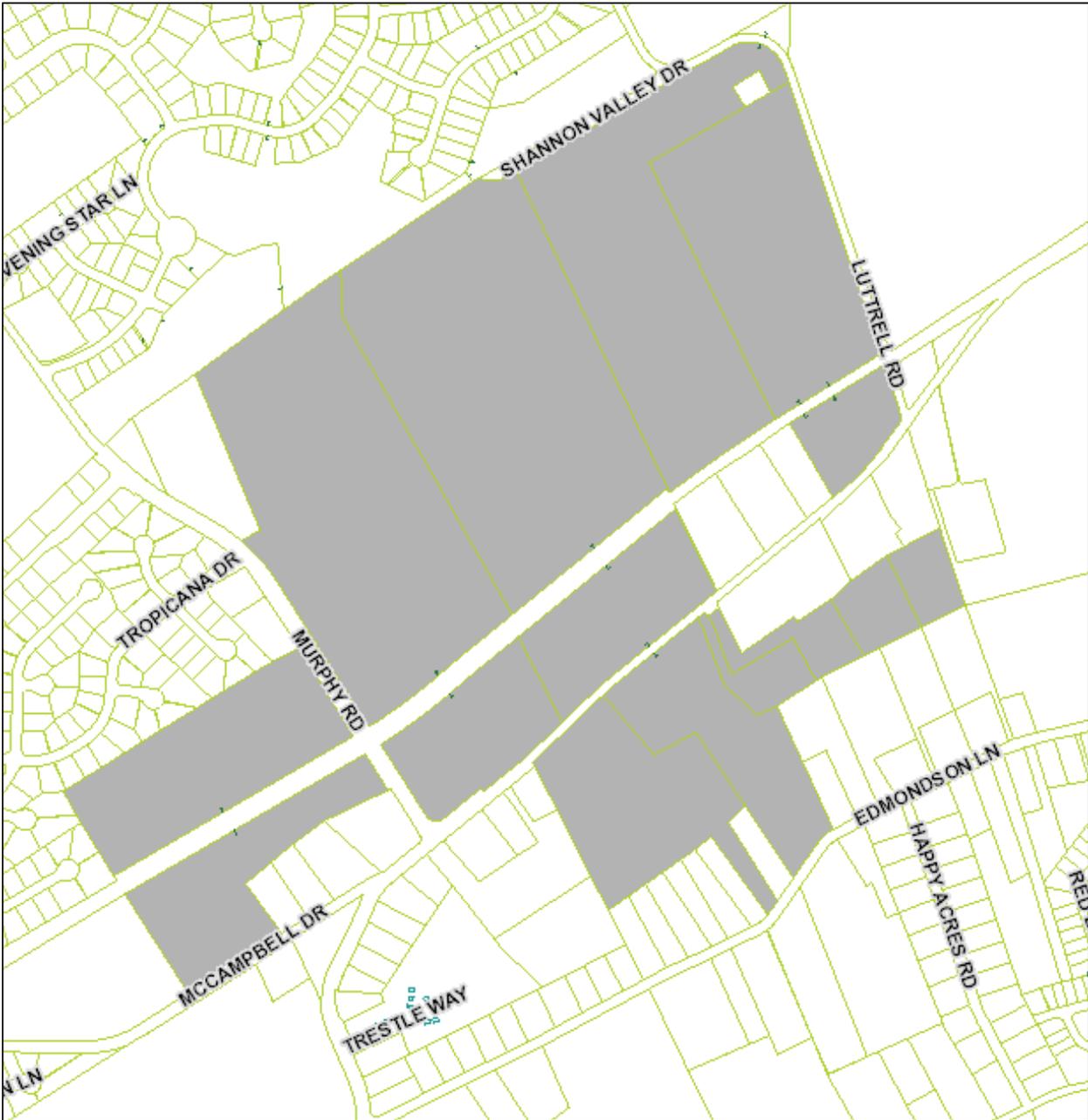


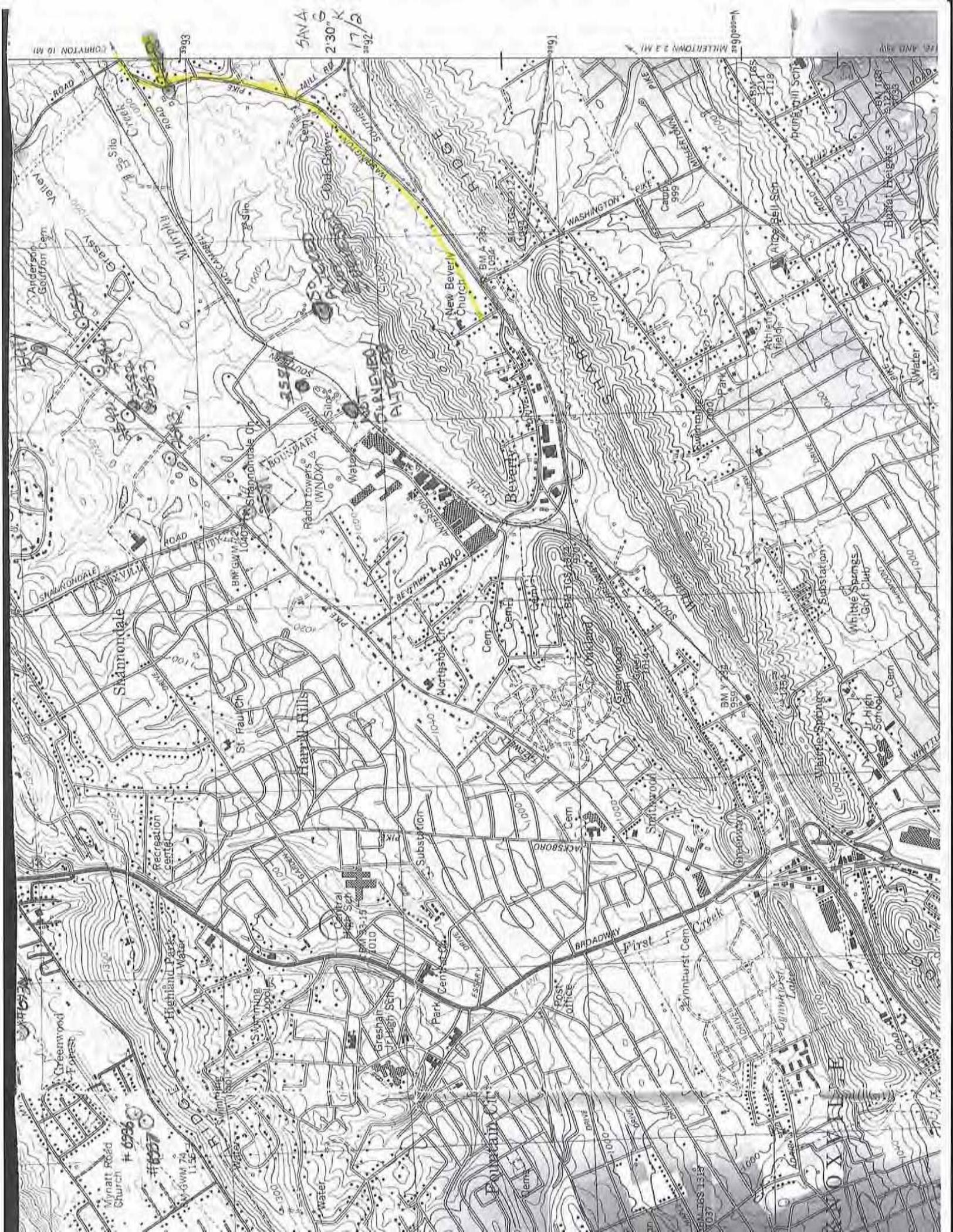
Figure 1 – Murphy Farm Map

The parcels that constitute the Murphy farm are:

Address	Parcel ID	Size (Acres)
5817 McCampbell Dr	049 08301	3.17
0 Murphy Rd	049 083	20.88
4508 Murphy Rd	049 080	49.50
0 Washington Pike	049 077	58.78
4671 Luttrell Rd	049 071	26.84
6029 Washington Pike	050 001	25.00
5922 Washington Pike	049 078	14.38
5930 Washington Pike	049 07701	2.25
5932 Washington Pike	049 07702	2.60
5936 Washington Pike	050 00201	2.41
0 Washington Pike	050 00202	2.11

Table 1 – Murphy Farm Parcels

Appendix C
Previous Surveys - Maps



SVA 2:30 K 17/8

Shannondale

Hazel Hill

Oxley

Poultney

New Beverly Church

Highland Park Recreation Centre

Mynatt Road Church

Central High Sch

Gresham Primary Sch

Post Office

First Creek

Symhurst Cem

White Springs Golf Club

White Springs Jr High School

White Springs Substation

White Springs Cem

Beaver Creek

St. Faulch

St. Ann's Ch

St. Mary's Ch

St. Joseph's Ch

St. Peter's Ch

St. Paul's Ch

St. James' Ch

St. John's Ch

St. George's Ch

St. Andrew's Ch

St. David's Ch

St. Elizabeth's Ch

St. Francis' Ch

St. Ignace's Ch

St. Joseph's Ch

St. Lawrence's Ch

St. Michael's Ch

St. Nicholas' Ch

St. Patrick's Ch

St. Raphael's Ch

St. Rose's Ch

St. Theresa's Ch

St. Vincent's Ch

St. Xavier's Ch

St. Yves' Ch

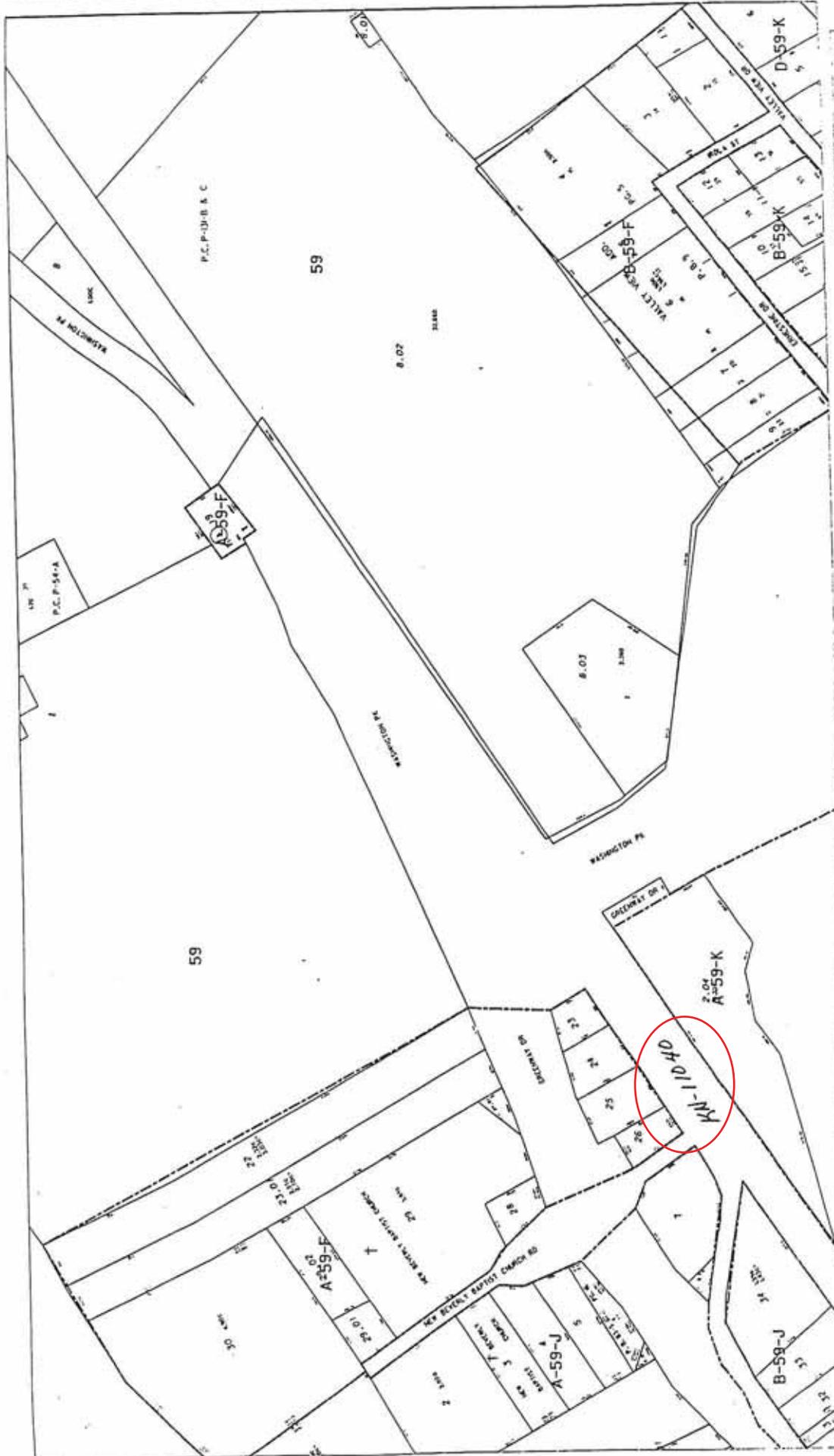
St. Zeno's Ch

St. Zenobius' Ch

St. Agatha's Ch

St. Agnes' Ch

St. Agostino's Ch



PROPERTY ASSESSOR'S OFFICE
 KNOX COUNTY TN
 MAP NO. 57-2
 DATE: 1-18-18
 REVISED: 1-18-18
 SHEET NO. 1

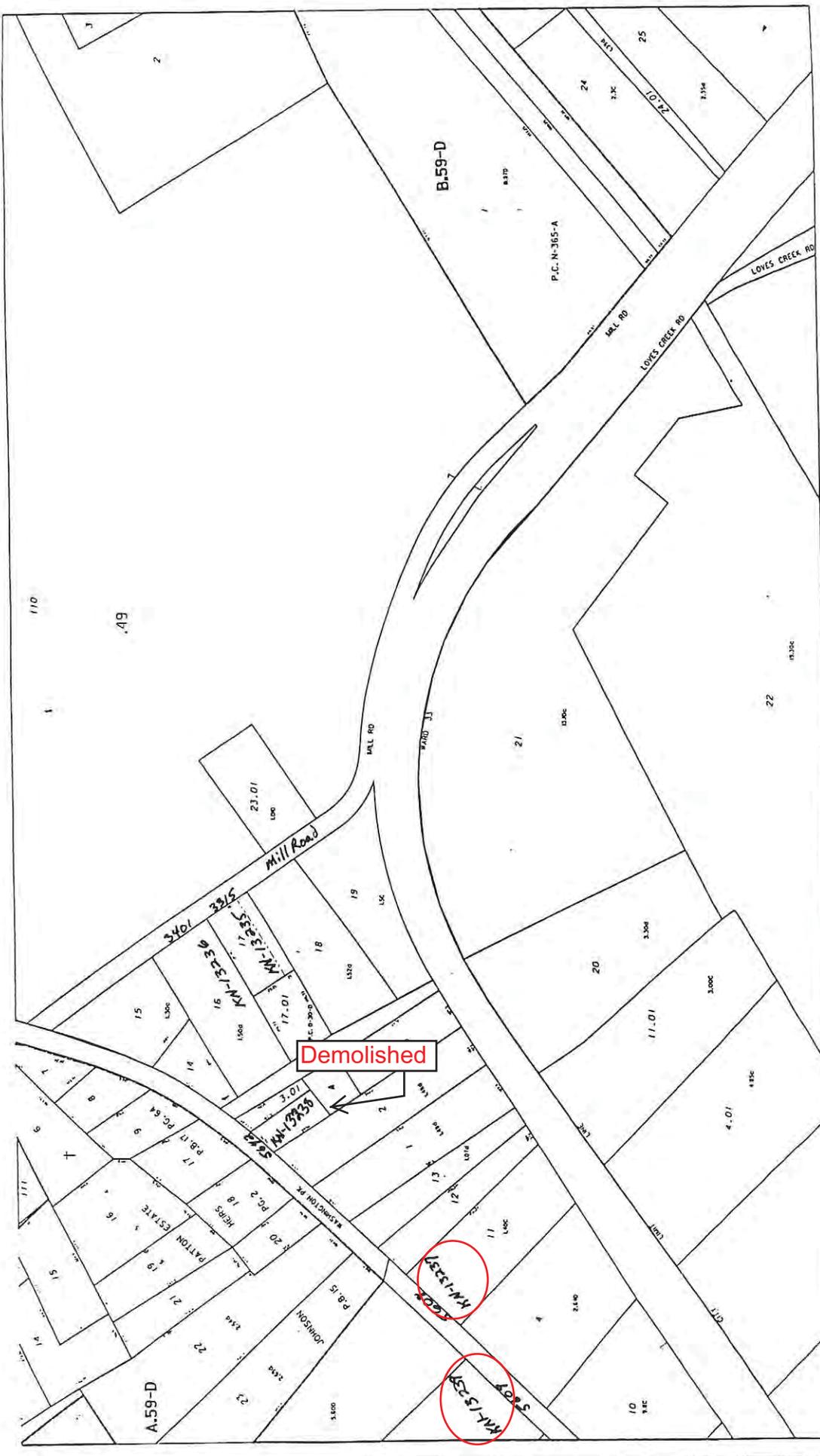
59-B	59-C	59-D
59-E	59-F	59-G
59-H	59-I	59-J
59-K	59-L	59-M



SYMBOL	DESCRIPTION
(Solid line)	EXISTING LOT LINES
(Dashed line)	PROPOSED LOT LINES
(Double line)	STREETS
(Single line)	ALLEYS
(Dotted line)	EASEMENTS
(Hatched area)	UNDEVELOPED AREAS

SEE P. 400





PROPERTY ASSESSORS OFFICE

BOOK	COUNTY	TR	MAP No.
SALE 1188	TR	DISTRICT NO. 15	59-D
PLATED DEC 1979			

Copyright 1989, Courthouse Record Systems, Inc.

45-N	49-M	50-P
59-C	59-D	60-A
59-F	59-E	60-H



- BOUNDARY
- SURVEYED AND CORRECTED
- FROM THE 1881
- CORNER
- ADJACENT PROPERTY
- IDENTIFICATION POINTS
- IDENTIFICATION POINTS

- BOUNDARY
- FROM THE 1881
- CORNER
- ADJACENT PROPERTY
- IDENTIFICATION POINTS
- IDENTIFICATION POINTS

- PROPERTY NUMBER
- PARCEL NUMBER
- LOT NUMBER
- CALCULATED AREA
- AREA
- AREA
- AREA
- AREA

SEE P. 407



Bean, Jana L

From: Joseph Garrison [Joseph.Garrison@tn.gov]
Sent: Tuesday, January 08, 2013 10:54 AM
To: Bean, Jana L
Subject: Survey Project along Washington Pike in Knox County.

Follow Up Flag: Follow up
Flag Status: Flagged

Ms. Bean,

Thank you for your recent email. I have checked my log and find that our office concurred with the TDOT in letters dated November 2, 2000 that there were no historic architectural resources located within that agency's proposed project Areas of Potential Effect along Washington and Millertown Pikes . That was 12 years ago, however, so any proposed projects with Areas of Potential Effects bounding Washington Pike must be treated by this office as new undertakings and re-surveyed for possible National Register properties.

Best,

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